

## Appendix E1:

### Section 106 Area of Potential Effects and Historic Properties Technical Report



# Long Bridge Project

## Environmental Impact Statement (EIS)

### Area of Potential Effects and Historic Properties Technical Report

February 23, 2018

# Long Bridge Project EIS

## *Area of Potential Effects and Historic Properties*

### *Technical Report*

---

## **Table of Contents**

<b>1.0</b>	<b>Introduction</b> .....	<b>1</b>
<b>2.0</b>	<b>APE Methodology</b> .....	<b>3</b>
2.1.	Section 106 and Virginia Department of Historic Resources (VDHR) Guidance .....	3
2.2.	Development of the APE.....	3
2.3.	Long Bridge Section 106 Consultation .....	7
2.4.	Field Survey Documentation.....	9
<b>3.0</b>	<b>Identification of Historic Properties</b> .....	<b>27</b>
3.1.	Designated Historic Properties .....	30
3.2.	Eligible Historic Properties .....	44
3.3.	Properties at or Greater than Forty-Five Years of Age .....	48

## List of Tables

Table 3-1   List of historic properties previously listed in the NRHP, DC Inventory, or VLR. Several of the below properties listed on the DC Inventory have also been determined eligible for listing on the NRHP. .....	30
Table 3-2   List of historic properties that have been determined eligible for listing in the NRHP by a Federal agency or recommended as eligible by a SHPO.....	44
Table 3-3   List of historic properties that have been determined eligible for listing in the NRHP by a Federal agency or recommended as eligible by a SHPO.....	48

## List of Figures

Figure 1-1   Long Bridge Project Area Limits .....	2
Figure 2-1   Potential Bike-Pedestrian Crossing Alignment Options.....	5
Figure 2-2   Map of APE, Limits of Disturbance, and Viewshed Sites .....	9
Figure 2-3   Map of Field Survey Locations .....	11
Figure 2-4   Representative Areas within the APE That Illustrate the Visibility of the Long Bridge Corridor .....	12
Figure 2-5   Map detail of photograph locations 1, 2, and 3.....	13
Figure 2-6   Photograph location 1. Long Bridge from the west end of the Roosevelt Bridge, facing southeast .....	13
Figure 2-7   Photograph location 2. Long Bridge from the west section of the Kennedy Center upper terrace, facing southeast .....	14
Figure 2-8   Photograph location 3. Long Bridge from the Lincoln Memorial public viewing platform, facing southeast.....	14
Figure 2-9   Map detail of photograph locations 4 and 5 at Arlington National Cemetery .....	15
Figure 2-10   Photograph location 4. Long Bridge from Arlington House, facing southeast .....	15
Figure 2-11   Photograph location 5. Long Bridge from the Tomb of the Unknown Soldier, facing west .	16
Figure 2-12   Map detail of photograph locations 6, 7, and 8 at George Washington Memorial Parkway, Gravelly Point, and Mount Vernon Trail. ....	16
Figure 2-13   Photograph location 6. Long Bridge from Mount Vernon Trail to the north of Arlington Memorial Bridge, facing southeast.....	17

Figure 2-14   Photograph location 7. Long Bridge from the Mount Vernon Trail to the north of I-395, facing southeast.....	17
Figure 2-15   Photograph location 8. Long Bridge from Gravelly Point, facing north.....	18
Figure 2-16   Map detail of photograph locations 9 and 10 at Reagan National Airport .....	18
Figure 2-17   Photograph location 9. Long Bridge from north boundary of Reagan Airport at the Potomac River, facing north.....	19
Figure 2-18   Photograph location 10. Long Bridge from the southern edge of the airport, facing north/northwest .....	19
Figure 2-19   Map detail of photograph locations 11 and 12, Joint Base Anacostia-Bolling .....	20
Figure 2-20   Photograph location 11. Long Bridge from Arnold Avenue, SW, facing northwest .....	20
Figure 2-21   Photograph location 12. Long Bridge to the west of Boundary Drive at the Anacostia River, facing northwest .....	21
Figure 2-22   Map detail of photograph location 14, St. Elizabeths West Campus .....	21
Figure 2-23   Photograph 2. Long Bridge from Saint Elizabeths West Campus, facing northwest .....	22
Figure 2-24   Map detail of photograph locations 14, 15, and 16, East Potomac Park, Hains Point, and Fort McNair.....	22
Figure 2-25   Photograph location 14. Long Bridge from Hains Point, facing northwest .....	23
Figure 2-26   Photograph location 15. Long Bridge Corridor from East Potomac Park at the Washington Channel, facing northwest.....	23
Figure 2-27   Photograph location 16. Long Bridge Corridor from Fort McNair at B Street SW, facing northwest.....	24
Figure 2-28   Map detail of photograph locations 17, 18, and 19.....	24
Figure 2-29   Photograph location 17. Long Bridge Corridor from Independence Avenue SW, and 14th Street SW facing south.....	25
Figure 2-30   Photograph location 18. Long Bridge Corridor from intersection of Independence Avenue SW and 9th Street SW, facing south .....	25
Figure 2-31   Photograph location 19. Long Bridge Corridor from intersection of Maryland Avenue SW, and 7th Street SW, facing southwest.....	26
Figure 3-1   Map of APE with Designated and Eligible Historic Properties .....	29
Figure 3-2   National Mall.....	31
Figure 3-3   Rock Creek and Potomac Parkway.....	32

**Long Bridge Project EIS**

Figure 3-4 | Rock Creek and Potomac Parkway..... 32

Figure 3-5 | George Washington Memorial Parkway (Mount Vernon)..... 32

Figure 3-6 | Mount Vernon Memorial Highway (Google Maps) ..... 33

Figure 3-7 | Detail, L’Enfant Plan Facsimile, 1887 (Library of Congress)..... 33

Figure 3-8 | Hains Point, East and West Potomac Parks Historic District..... 33

Figure 3-9 | Long Bridge ..... 34

Figure 3-10 | Jefferson Memorial (National Park Service) ..... 34

Figure 3-11 | Central Heating Plant ..... 34

Figure 3-12 | USDA Cotton Annex ..... 35

Figure 3-13 | HUD Building ..... 35

Figure 3-14 | USDA South Building ..... 35

Figure 3-15 | BEP Main Building ..... 36

Figure 3-16 | Auditor’s Building (Library of Congress) ..... 36

Figure 3-17 | Memorial Bridge ..... 36

Figure 3-18 | Fort McNair (National Defense University)..... 37

Figure 3-19 | Titanic Memorial ..... 37

Figure 3-20 | Lunch Room ..... 37

Figure 3-21 | Cuban Friendship Urn (Wikimedia Commons)..... 38

Figure 3-22 | Roosevelt Memorial (National Park Service) ..... 38

Figure 3-23 | Johnson Memorial Grove (National Park Service) ..... 39

Figure 3-24 | Lincoln Memorial (National Park Service)..... 39

Figure 3-25 | Washington Monument and Grounds (National Park Service) ..... 40

Figure 3-26 | Arlington House (National Park Service)..... 40

Figure 3-27 | Arlington National Cemetery (Arlington Cemetery) ..... 41

Figure 3-28 | St. Elizabeths West Campus ..... 41

Figure 3-29 | The Netherlands Carillon (National Park Service)..... 42

Figure 3-30   The Old Post Office (National Park Service).....	42
Figure 3-31   The Pentagon (VDHR) .....	43
Figure 3-32   BEP Annex .....	44
Figure 3-33   FOB 10A (GSA).....	45
Figure 3-34   Banneker Park .....	45
Figure 3-35   Richmond, Fredericksburg and Potomac Railroad (Richmond, Fredericksburg & Potomac Railroad Historical Society, Inc.) .....	45
Figure 3-36   Washington Marina Building .....	46
Figure 3-37   L'Enfant Promenade.....	46
Figure 3-38   Lady Bird Johnson Park (Cultural Landscape Foundation).....	46
Figure 3-39   Kennedy Center (Wikimedia Commons).....	47
Figure 3-40   Liberty Loan Federal Building (Google Maps) .....	47
Figure 3-41   425 12th Street, SW (Google Maps) .....	48
Figure 3-42   Astral Building (Google Maps) .....	49
Figure 3-43   Comsat Building (Google Maps).....	49
Figure 3-44   Loew's L'Enfant Plaza Hotel (Google Maps) .....	50
Figure 3-45   USPS Building (Google Maps).....	50
Figure 3-46   398 Long Bridge Drive (Google Maps) .....	51

## 1.0 Introduction

The Federal Railroad Administration (FRA) and District Department of Transportation (DDOT) are concurrently preparing an Environmental Impact Statement (EIS) in accordance with the National Environmental Policy Act (NEPA), and an assessment of effects on historic properties per Section 106 of the National Historic Preservation Act (NHPA) for the Long Bridge Project (the Project). The Long Bridge Project consists of potential improvements to the Long Bridge and related railroad infrastructure located between the Rosslyn (RO) Interlocking near Long Bridge Park in Arlington, Virginia, and the L'Enfant (LE) Interlocking near 10<sup>th</sup> Street SW in the District (the Long Bridge Corridor). The Long Bridge Corridor is shown in Figure 1-1.

The purpose of the Proposed Action is to provide additional long-term railroad capacity and to improve the reliability of railroad service through the Long Bridge Corridor. Currently, there is insufficient capacity, resiliency, and redundancy to accommodate the projected demand in future railroad services. The Proposed Action is needed to address these issues and to ensure the Long Bridge Corridor continues to serve as a critical link connecting the local, regional, and national transportation network.

Although not part of the Proposed Action's Purpose and Need, the Project will explore the potential opportunity to accommodate connections that follow the trajectory of the Long Bridge Corridor to the pedestrian and bicycle network. The feasibility of this opportunity will be assessed as the Project progresses and will consider whether a crossing can be designed to be consistent with railroad operator plans and pursuant to railroad safety practices. Future efforts to accommodate connections to the pedestrian and bicycle network may be advanced as part of the Project, or as part of a separate project(s) sponsored by independent entities.

This report outlines the methodology for delineating and refining the Area of Potential Effects (APE) in accordance with Section 106 of the NHPA (54 U.S.C. § 300101 *et seq.*)<sup>1</sup> and its implementing regulations (36 CFR Part 800) for the Project.<sup>2</sup>

This report includes the following:

1. A description of the methodology used to delineate the APE;
2. Results of the field survey completed to inform APE development; and
3. An identification of historic properties as well as properties at or greater than 45 years of age that may be affected by the Long Bridge Project.

---

<sup>1</sup> 54 USC 300101, National Park Service and Related Programs, National Preservation Programs, Division A-Historic Preservation [http://uscode.house.gov/view.xhtml?req=\(title:54%20section:300101%20edition:prelim\)](http://uscode.house.gov/view.xhtml?req=(title:54%20section:300101%20edition:prelim))

<sup>2</sup> 36 CFR Part 800, Protection of Historic Properties, <http://www.achp.gov/regs-rev04.pdf>.



**Figure 1-1** | Long Bridge Project Area Limits



## 2.0 APE Methodology

### 2.1. Section 106 and Virginia Department of Historic Resources (VDHR) Guidance

The Section 106 regulations define an APE as, "...the geographic area within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The APE is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking" (36 CFR 800.16[d])<sup>1</sup>. The APE is defined to allow for the evaluation of potential effects to historic properties resulting from an undertaking. According to the steps prescribed by the Section 106 regulations, the APE must be defined before the identification of historic properties and evaluation of potential effects occurs. Types of effects on historic properties may include:

- Direct (such as physical destruction, damage, relocation, or alteration of a property);
- Indirect (such as introduction of visual, atmospheric, or audible elements that diminish the integrity of a property's significant historic features);
- Temporary;
- Future; and
- Cumulative.

Adverse effects occur when an undertaking may directly or indirectly alter characteristics of a historic property that qualify it for inclusion in the National Register of Historic Places. Examples of adverse effects are stated in 36 CFR Part 800.5(a)(2). Adverse effects have the potential to occur both during the construction and operational periods of a project.

For each undertaking, the Section 106 regulations (36 CFR Part 800) require the lead Federal agency to determine an APE boundary that considers multiple types of effects on historic properties, rather than multiple APEs that address various effects. However, non-contiguous APEs may be developed to include multiple alternative project areas or multiple areas where possible effects may be reasonably anticipated. The regulations also require the lead Federal agency seek information from consulting parties and others likely to have knowledge of, or concerns with, historic properties in the area, to identify issues relating to the undertaking's potential effects on historic properties.

The VDHR provides guidance on APE development, requiring the APE to include all locations where the project will cause ground disturbance, all locations from which the project may be visible or audible, and all locations where the project may result in changes to land use, public access, traffic patterns, etc.<sup>3</sup> The DC Historic Preservation Office (DCSHPO) does not offer comparable guidance.

### 2.2. Development of the APE

The APE for the Long Bridge Project was delineated to identify and document the areas from which the Project could result in ground disturbance or could be reasonably visible or audible. Assumptions for the area within which the alternatives could be located were identified based on the results of Level 1 Concept Screening presented to the public and agencies in May 2017. Level 1 Concept Screening

---

<sup>3</sup> VDHR, *Defining Your Area of Potential Effects*, [http://www.dhr.virginia.gov/pdf\\_files/Defining\\_Your\\_APE.pdf](http://www.dhr.virginia.gov/pdf_files/Defining_Your_APE.pdf).

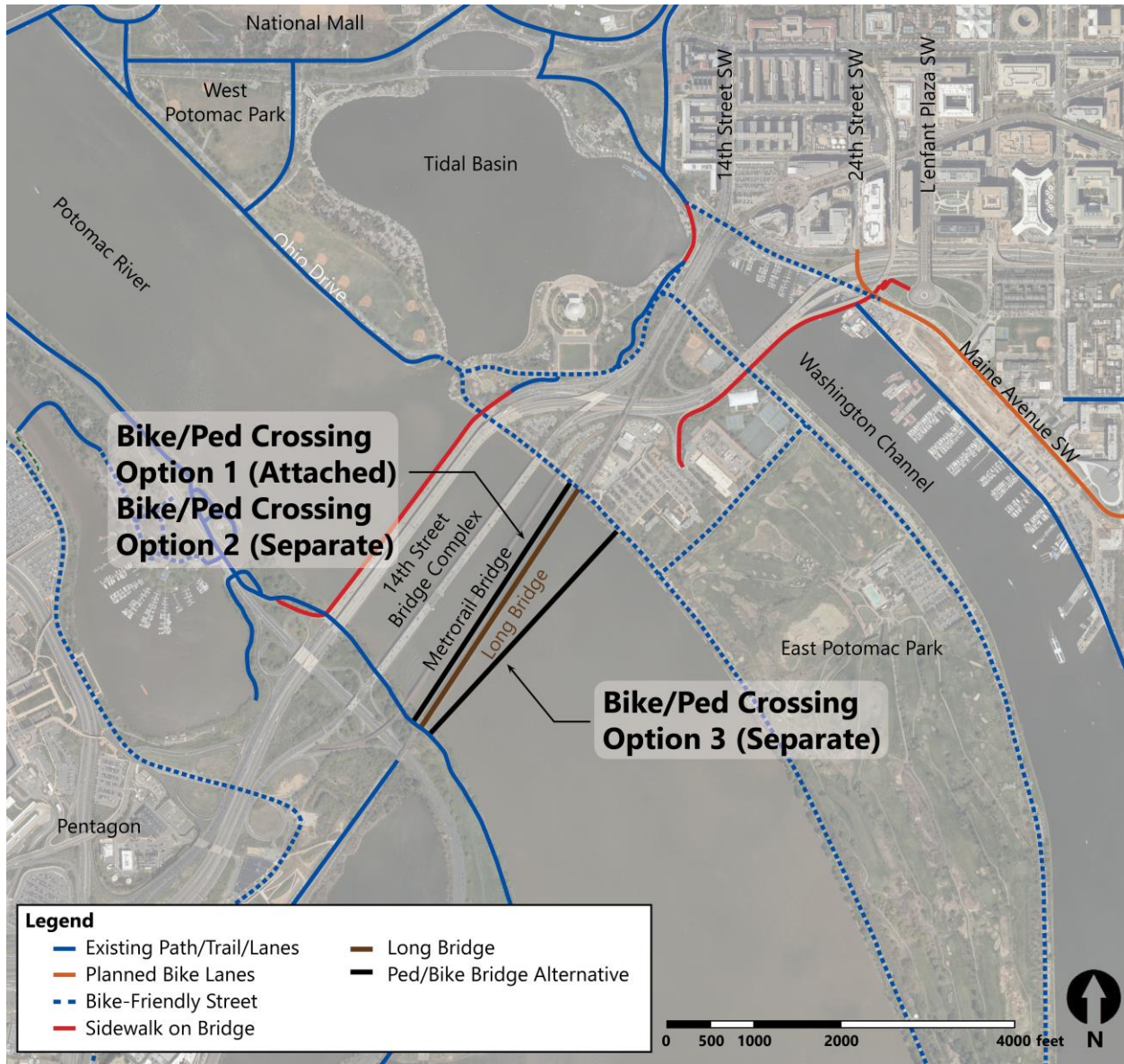
assessed preliminary concepts by their ability to meet the Project's Purpose and Need based on railroad capacity, transportation network connectivity, and railroad resiliency and redundancy. The 6 concepts found to meet Purpose and Need, as a result of Level 1 Screening were:

- 3-track crossing
- 3-track crossing with bike-pedestrian crossing
- 4-track crossing
- 4-track crossing with bike-pedestrian crossing
- 5-track crossing
- 5-track crossing with bike-pedestrian crossing

**These concepts all occur within the existing Long Bridge Corridor. Only above ground crossings (bridges) were found to meet Purpose and Need because a freight tunnel could not feasibly connect to existing freight infrastructure, and a passenger-only tunnel would not improve redundancy. The concepts vary in terms of the number of tracks and whether or not a bike-pedestrian crossing is included. Because of the need for any new bridge to tie back into the existing railroad corridor (network connectivity), all concepts would be constructed within a relatively tight band either within the current Long Bridge alignment, or upstream or downstream of the current alignment. The opportunity is currently being explored to provide a bike-pedestrian connection on a new railroad bridge, or on a separated structure upstream or downstream of a railroad bridge. Upstream bike-pedestrian alignments are constrained by the Metrorail bridge, while downstream alignments would need to avoid a Department of Defense Facility in East Potomac Park, and would therefore land close to the NPS headquarters building. Therefore, the outer limits of the potential Limits of Disturbance are set by the bike-pedestrian crossing alignment options, as depicted in**

Figure 2-1.

**Figure 2-1 | Potential Bike-Pedestrian Crossing Alignment Options**



The APE and Limits of Disturbance boundaries were mapped two dimensionally, although it was assumed that the boundaries encompass both above-ground and below-ground resources, including potential underwater and archaeological resources.

### **The Limits of Disturbance boundary (**

Figure 2-2, black dashed line) represents the area within which the Project has the potential to directly alter an existing feature or result in ground-disturbing activities.<sup>4</sup> Along the span of the existing Long Bridge and on NPS land on either side of the Potomac River, the Limits of Disturbance includes potential realignments of the existing railroad bridge in addition to potential bike and pedestrian crossings. These potential bridge alignments extend from the existing Metrorail Bridge to a distance of approximately 500 feet to the southeast. Additionally, the Limits of Disturbance extend outward from these points on the east and west banks of the Potomac, at a distance of approximately 250-300 feet, to incorporate associated bike-pedestrian access ramps on each side. Along the remainder of the Long Bridge corridor, the Limits of Disturbance includes a buffer of approximately 50' on either side of the existing corridor centerline between RO and LE Interlockings.

### **The APE (**

Figure 2-2, red dashed line) represents areas from which atmospheric or environmental changes are possible. The methodologies used to develop the APE included:

- Digital mapping and aerial photography to guide and supplement field data;
- The impact of topographic and other vertical changes (such as buildings and viewing platforms) and their effect on potential views and viewsheds, including sightlines from various locations in and surrounding the National Mall and wider viewsheds in areas along the banks of the Potomac River; and
- Windshield-level field surveys around the Project Area to determine the visibility of the Project, based on height of the existing Long Bridge steel trestle and component bridge, abutment, and track structures.<sup>5</sup>

---

<sup>4</sup> The LOD is defined as the geographic area(s) within which ground disturbance is anticipated to occur resulting from a specific project. It is developed to better understand the potential effects to archaeological resources within the APE. For the Long Bridge Project, once FRA the LOD may be refined, in consultation with SHPOs, as project engineering progresses by the size and location of bridge piers, abutments, etc. and the associated limits of ground disturbance.

<sup>5</sup> Visibility of the existing Long Bridge Project area was generally used as a determinant of the delineation of the APE boundaries over potential effects resulting from sound and vibration. Sound diminishes as a function of distance at a higher rate than light. An object further away could still be seen but may not be heard; or could be heard to a small degree that would not cause adverse effects. Therefore, changes to views and viewsheds resulting from Project implementation will have the greatest potential to affect historic properties. Additionally, permanent changes in sound regularity or intensity are not anticipated; however, there may be temporary effects during construction.

The process to evaluate the affected environment for noise and vibration will include identifying noise and vibration-sensitive receptors, understanding the predominant sources of noise and vibration, and characterizing existing noise and vibration conditions through measurements and modeling. This process will be conducted concurrently with the EIS studies, and the findings will be incorporated into the delineation of the final APE and in the assessment of effects on historic properties.

Therefore, although other indirect effects (such as audial changes) have been considered, there is a lesser potential for these effects to influence the outer boundaries of the APE. At the time in the Section 106 process when adverse effects are identified, it will be necessary to use available engineering data to quantify and evaluate the potential adverse effects associated with temporary and permanent

Field survey photographs led to the identification of viewshed locations outside of the contiguous APE boundary. The field survey and photographs were used to determine visibility of the Long Bridge from specific viewshed vantage points. The selection of the viewshed sites was informed by several factors. Viewshed sites are areas from which the project area was clearly visible from a specific exterior vantage point or publicly accessible plaza or viewing platform. However, the view was sufficiently limited in these locations to not warrant expanding the APE to encompass the entirety of each site (for example, the Long Bridge was visible from Arlington House and the Tomb of the Unknown Soldier but not the entirety of Arlington Cemetery). Interiors of buildings were excluded from consideration. All viewshed sites are also historic properties, so there may be potential for impacts to these properties from the implementation of the Long Bridge Project. The viewsheds identified (

Figure 2-2) include:

- The Kennedy Center
- The Washington Monument
- The Lincoln Memorial
- St. Elizabeths West Campus
- Arlington Cemetery, Tomb of the Unknown Soldier
- Arlington House<sup>6</sup>
- Netherland Carillon (within Arlington Ridge Park)
- The Old Post Office Tower
- The Pentagon<sup>7</sup>

Future refinement of the APE will include:

- Reconsidering and adjusting the Limits of Disturbance boundary as EIS alternatives are further refined;<sup>8</sup>
- Incorporating future noise and vibration analysis findings; and
- Accounting for any additional feedback from DCSHPO and VDHR.

### **2.3. Long Bridge Section 106 Consultation**

The first Section 106 consulting parties meeting for the Long Bridge Project was held on April 25, 2017 at the DDOT offices. The attendees provided preliminary guidance for the development of an APE in the context of the preliminary project concepts presented. The comments received indicated a preference

---

impacts resulting from the project. Temporary impacts may include construction noise and vibrations; permanent impacts may include increased railroad traffic noise and vibration.

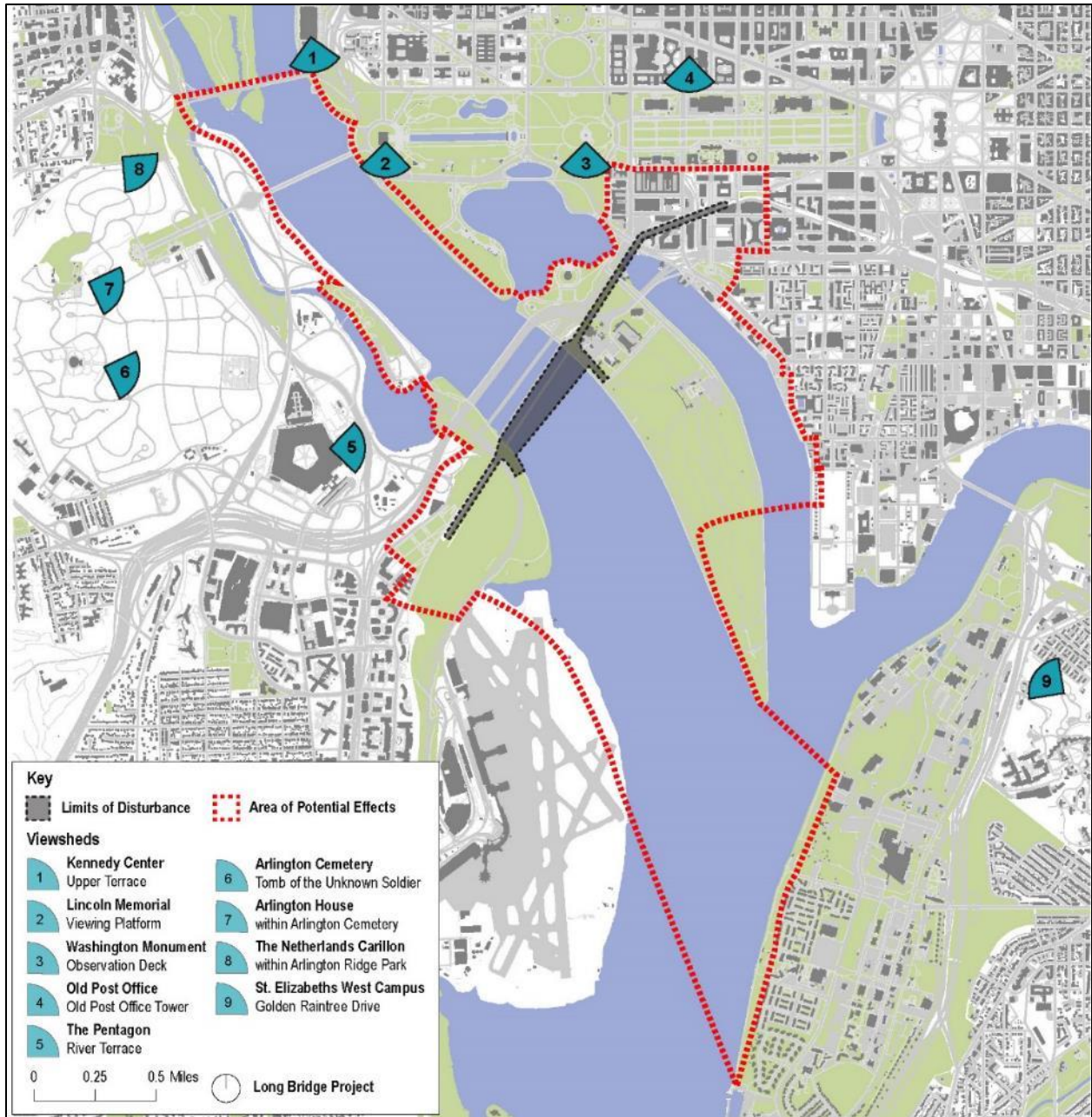
<sup>6</sup> Arlington House is located within the boundaries of Arlington National Cemetery. It is not administered by Arlington Cemetery but rather separately administered by the National Park Service.

<sup>7</sup> Site visits and field surveys photographs were taken from several additional viewshed points from which Long Bridge was either not visible. These sites include the Air Force Memorial, the Marine Corps War Memorial, at ground level at Arlington Ridge Park, the Washington National Airport historic terminal, and the Pentagon Metro Station.

for a single, comprehensive APE inclusive of all possible project alternatives (including options for potential bicycle and pedestrian access that follows the trajectory of the Long Bridge Corridor); that considers multiple types of effects (direct and indirect); and is sufficiently sized to accommodate the expansive and uninterrupted views along the Potomac River to the Long Bridge Corridor. Following the meeting, FRA and DDOT provided the Consulting Parties with a comment period ending May 9, 2017.

The second Section 106 consulting parties meeting was held on November 15, 2017 at the DCSHPO office. At this meeting, FRA and DDOT presented Draft APE and Limits of Disturbance boundaries in addition to the preliminary identification of historic properties. The attendees provided comments on the historic property identification, additional viewshed sites from which the Project area is visible, potential archaeological resources, and the graphic representation of the APE. FRA and DDOT incorporated those comments into the findings of this report. Following the meeting, FRA and DDOT provided the Consulting Parties with a comment period ending December 6, 2017.

**Figure 2-2 |** Map of APE, Limits of Disturbance, and Viewshed Sites



## 2.4. Field Survey Documentation

To establish preliminary boundaries for the APE, Esri ArcGIS and Google Maps were used to identify reasonable outer extents for a potential APE boundary. These reasonable outer extents included areas



of higher elevation (from which views would be more likely); major roadways (particularly elevated highways that would have a greater potential to block views); and other urban conditions like building density, street patterns, tree coverage, and potential viewsheds.

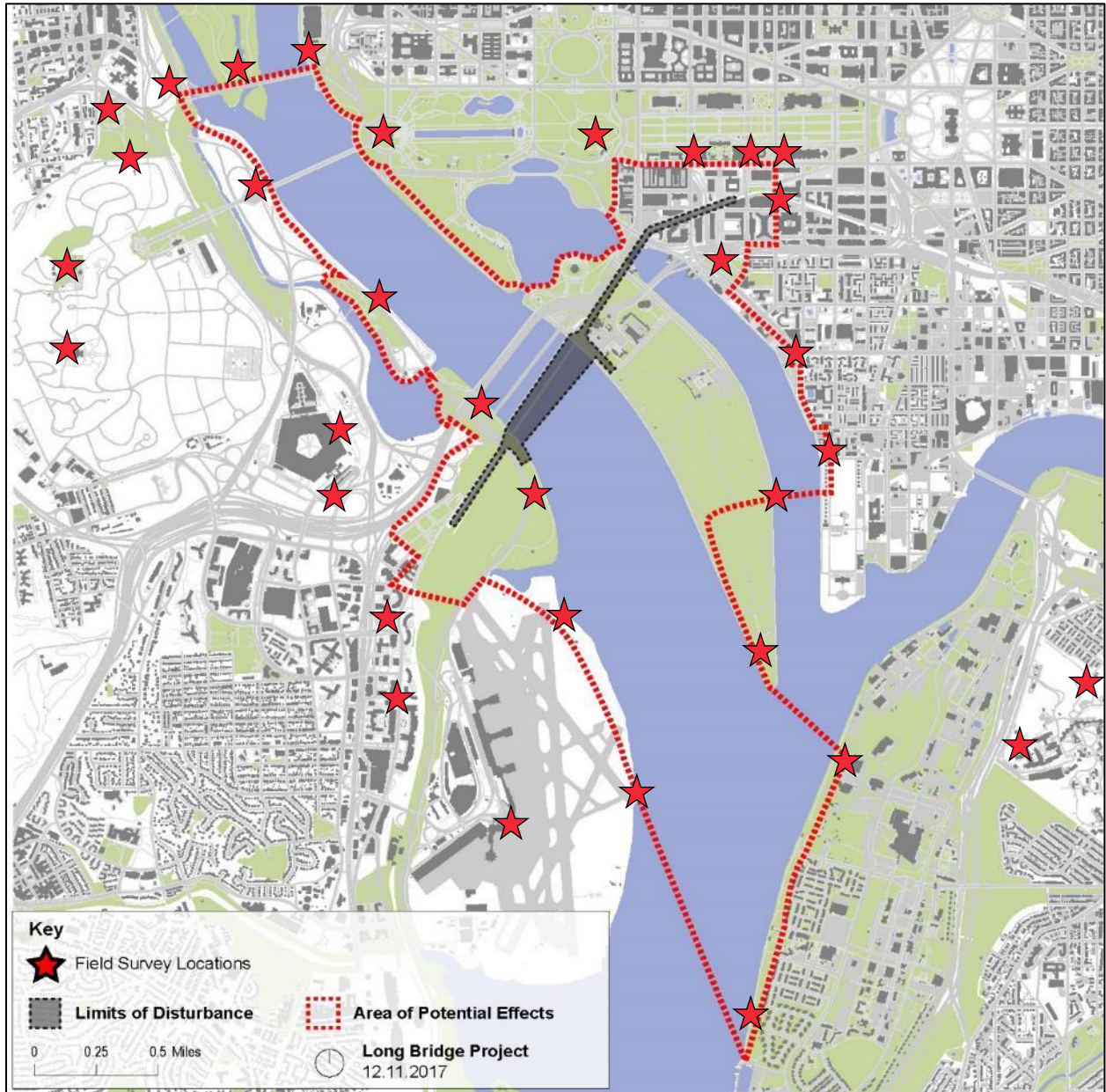
Impacts of topographic and other vertical changes, effects on potential views and viewsheds, and sightlines were tested by visiting specific viewing locations and viewing platforms. The existence of views toward the Long Bridge and the Long Bridge Corridor were recorded in field notes and digital photography. Exteriors of buildings and sites (such as the Kennedy Center upper and lower terraces) were also visited to confirm the visibility of the Long Bridge from these points.

The windshield survey was conducted to establish the outer boundaries of the Draft APE. Ten separate field surveys (on June 30, July 3, September 14, September 15, September 19, September 22, November 6, November 28, December 1, and December 5, 2017) were conducted to test and document the visibility of the Long Bridge Project from multiple and various geographic areas. The locations of these field survey points are documented in Figure 2-3.

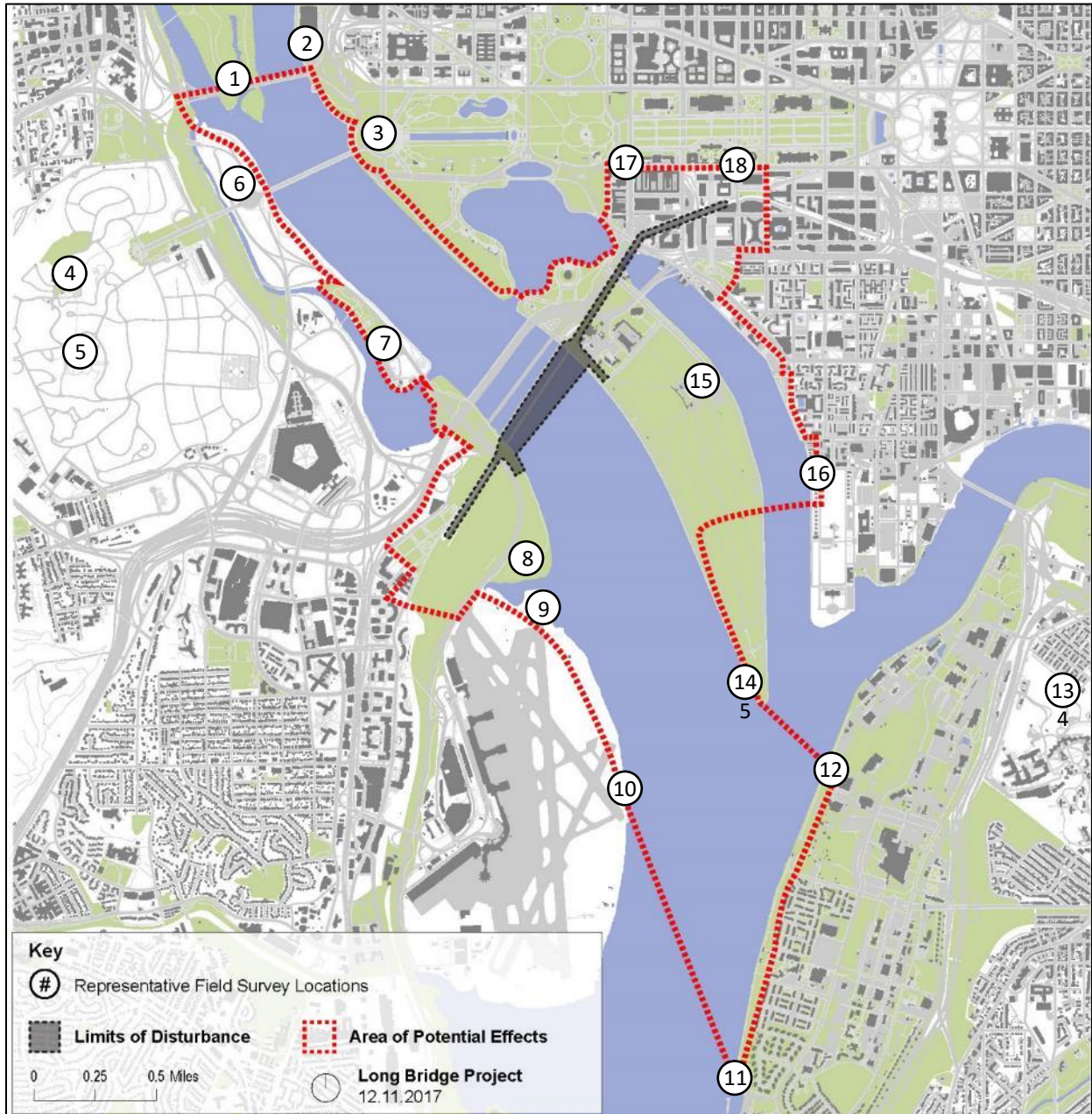
**The field survey locations indicated in Figure 2-3 are points chosen as representative areas within the APE that illustrate visibility of the Long Bridge Corridor. These points are distributed geographically across the APE. These areas are shown in further detail with accompanying supporting maps and photographs to depict views of the Long Bridge in**

Figure 2-4 through Figure 2-31. Site visits and field surveys photographs were taken from several additional viewshed points from which the Long Bridge was not visible. These sites include the Air Force Memorial, the Marine Corps War Memorial, at ground level at Arlington Ridge Park, the Washington National Airport historic terminal, and the Pentagon Metro Station.

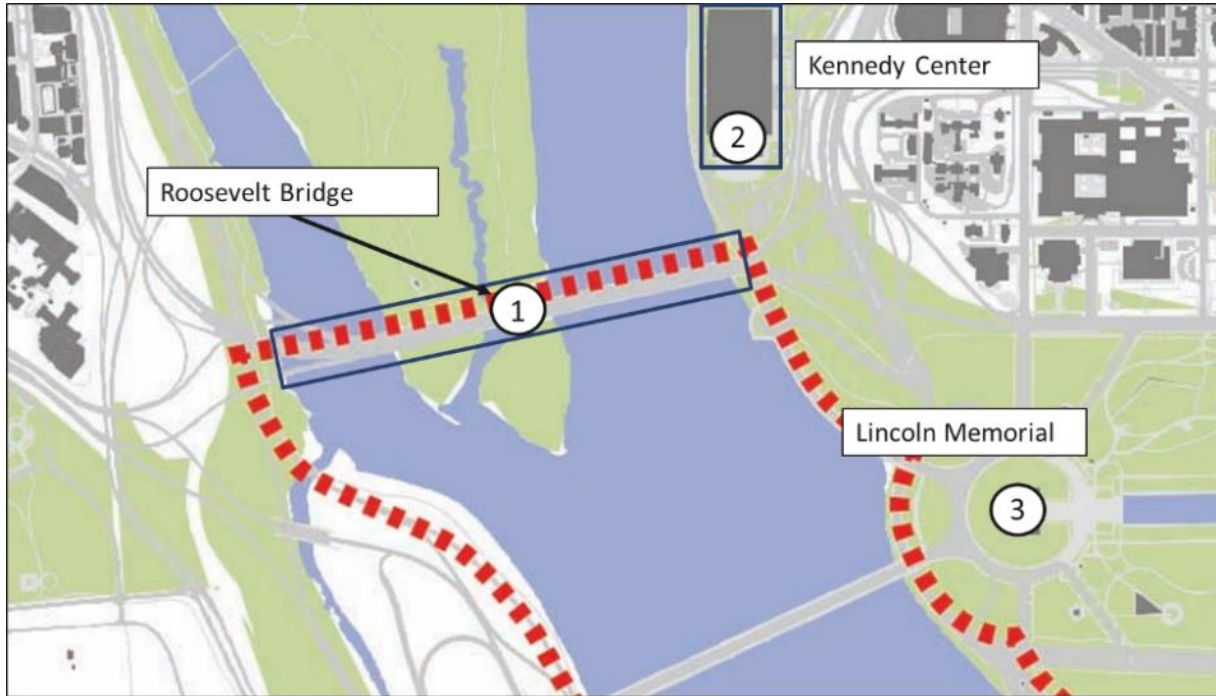
**Figure 2-3 |** Map of Field Survey Locations



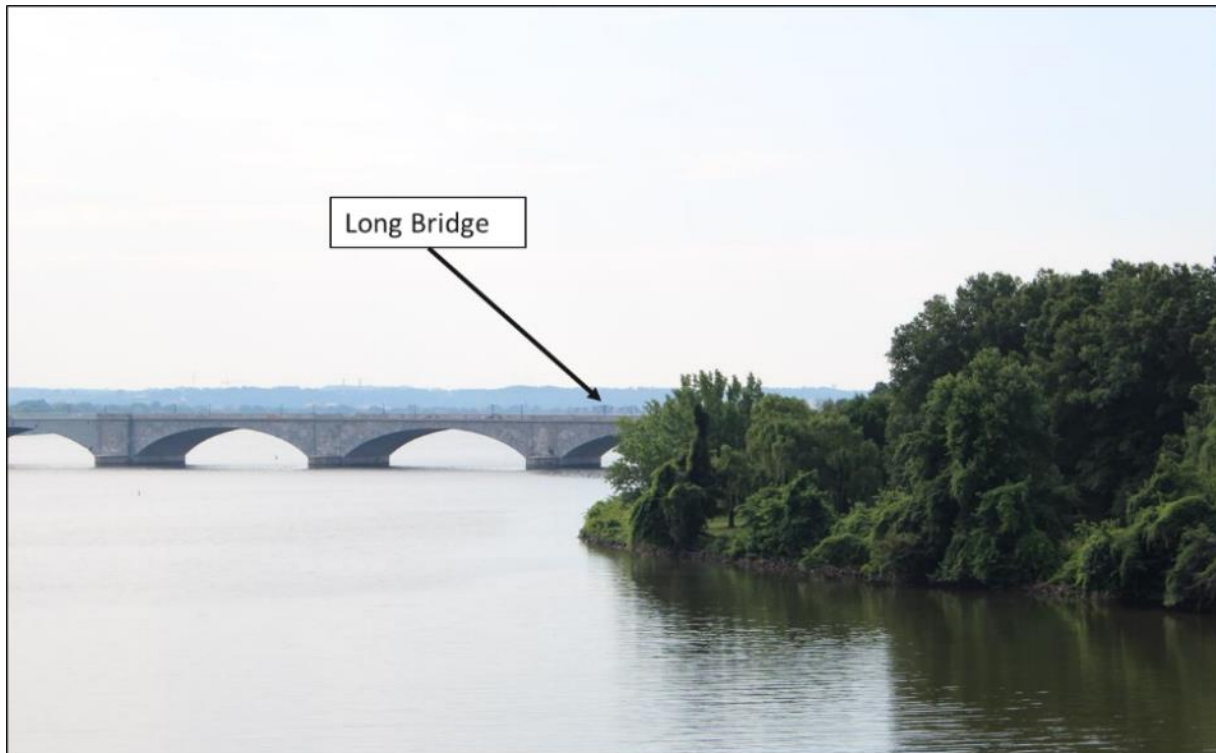
**Figure 2-4** | Representative Areas within the APE That Illustrate the Visibility of the Long Bridge Corridor



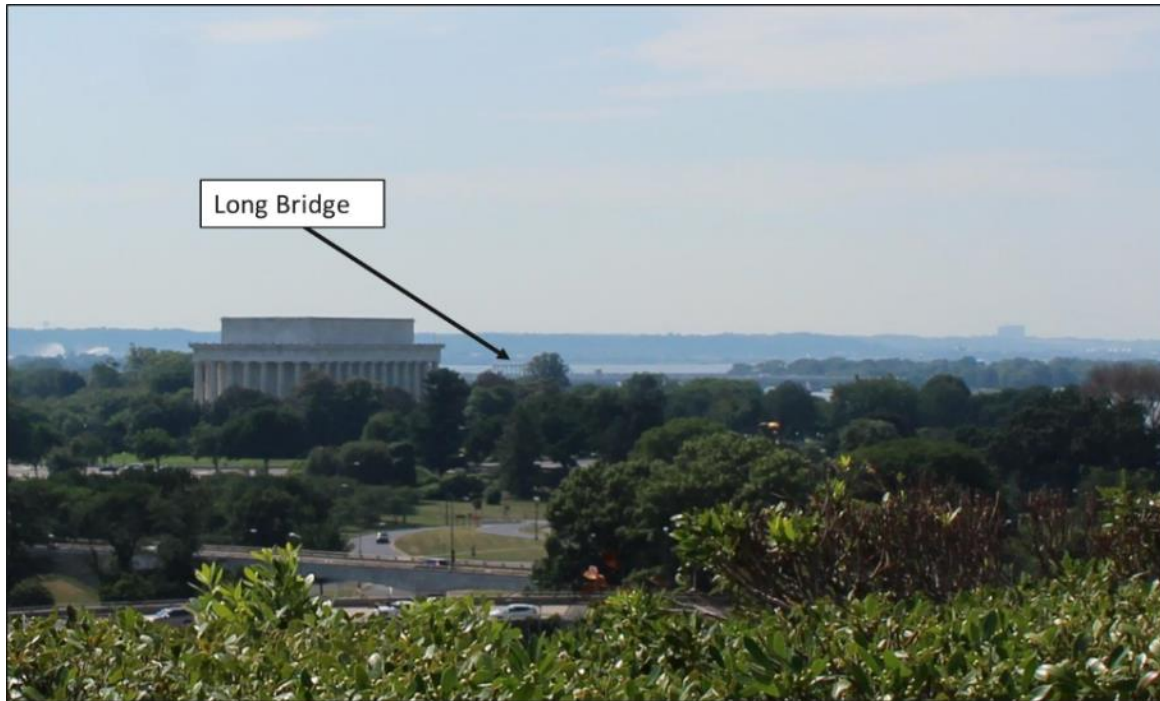
**Figure 2-5** | Map detail of photograph locations 1, 2, and 3



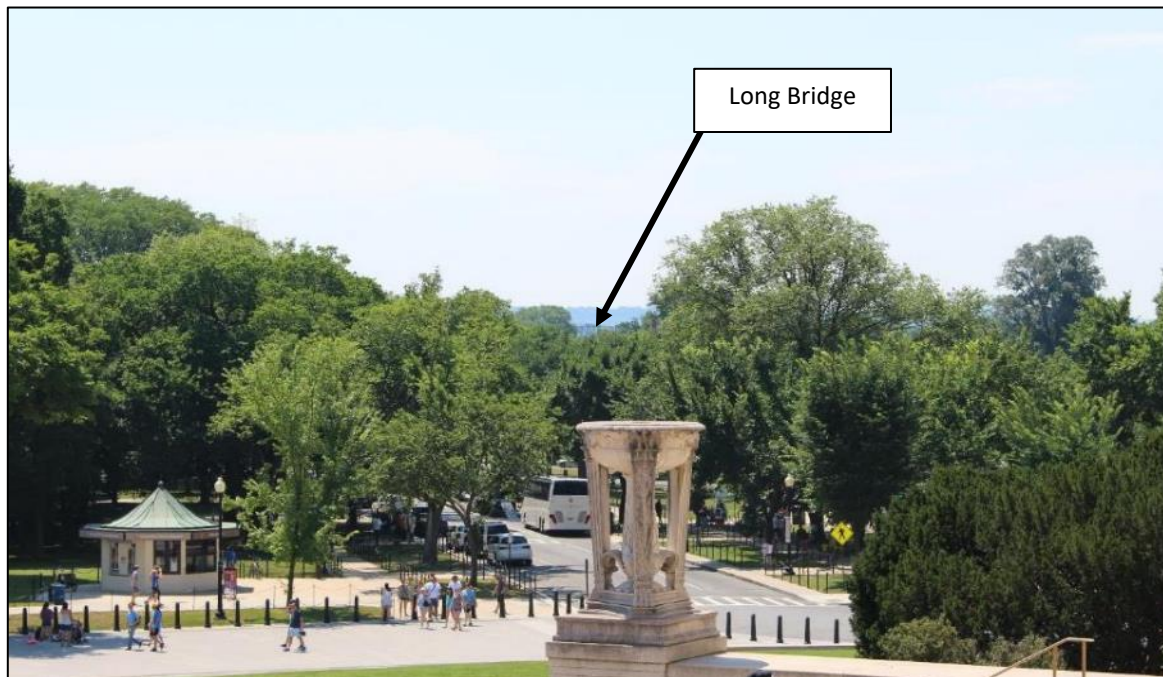
**Figure 2-6** | Photograph location 1. Long Bridge from the west end of the Roosevelt Bridge, facing southeast



**Figure 2-7** | Photograph location 2. Long Bridge from the west section of the Kennedy Center upper terrace, facing southeast



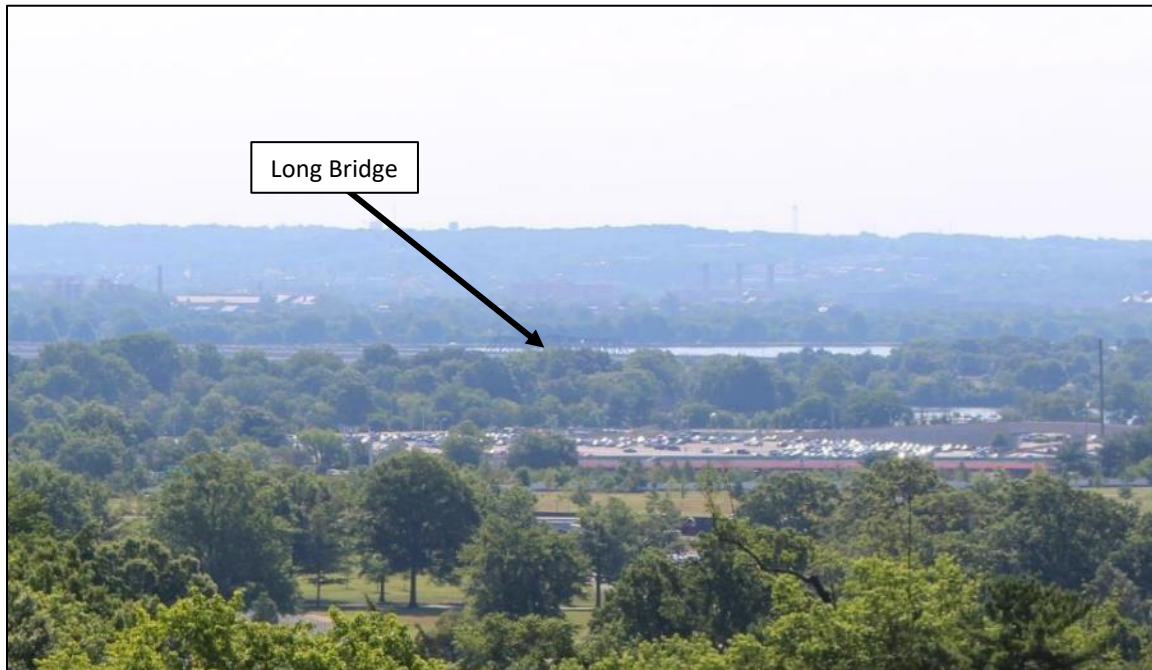
**Figure 2-8** | Photograph location 3. Long Bridge from the Lincoln Memorial public viewing platform, facing southeast



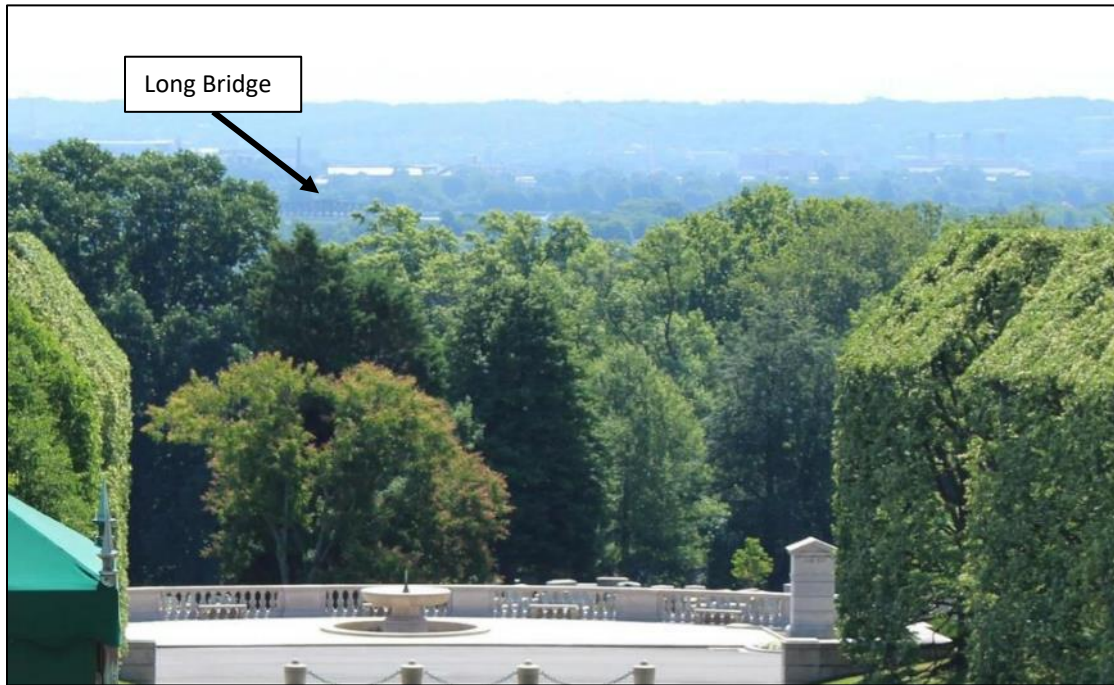
**Figure 2-9** | Map detail of photograph locations 4 and 5 at Arlington National Cemetery



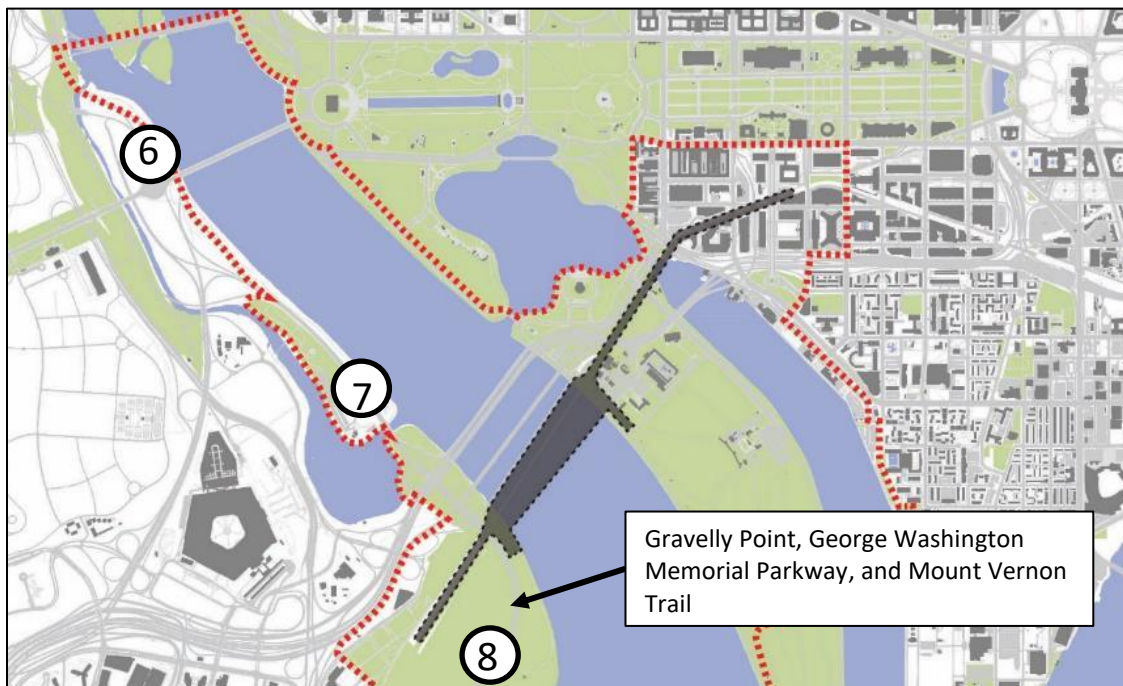
**Figure 2-10** | Photograph location 4. Long Bridge from Arlington House, facing southeast



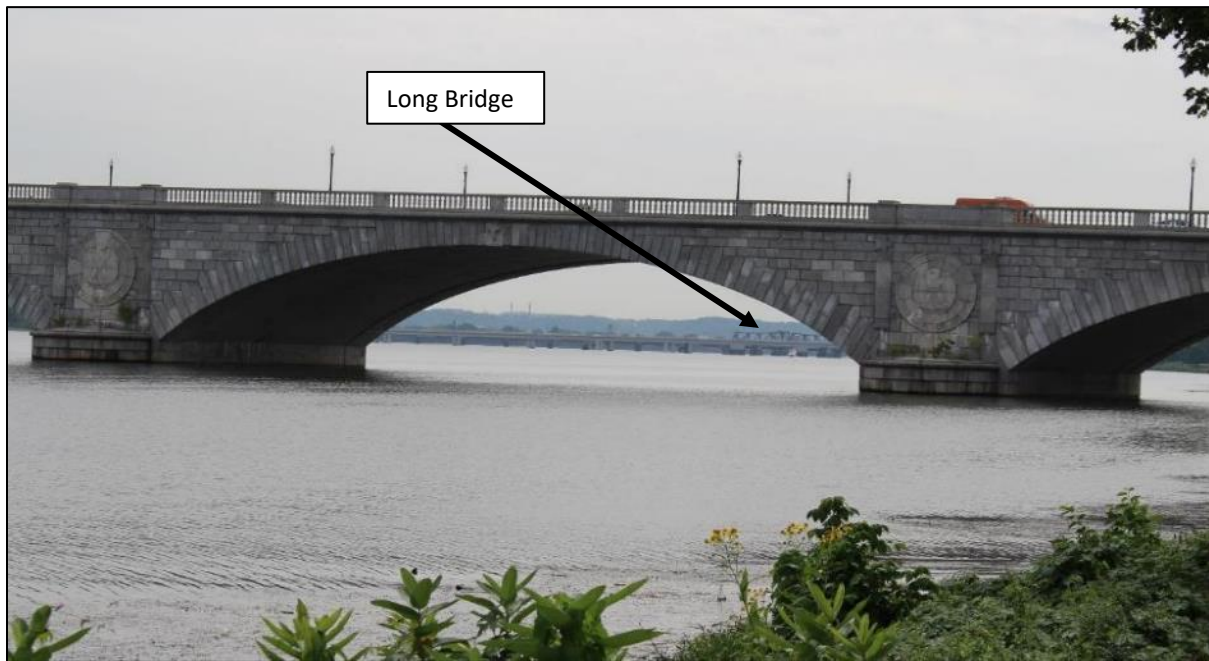
**Figure 2-11** | Photograph location 5. Long Bridge from the Tomb of the Unknown Soldier, facing west



**Figure 2-12** | Map detail of photograph locations 6, 7, and 8 at George Washington Memorial Parkway, Gravelly Point, and Mount Vernon Trail.



**Figure 2-13** | Photograph location 6. Long Bridge from Mount Vernon Trail to the north of Arlington Memorial Bridge, facing southeast



**Figure 2-14** | Photograph location 7. Long Bridge from the Mount Vernon Trail to the north of I-395, facing southeast





**Figure 2-15** | Photograph location 8. Long Bridge from Gravelly Point, facing north



**Figure 2-16** | Map detail of photograph locations 9 and 10 at Reagan National Airport



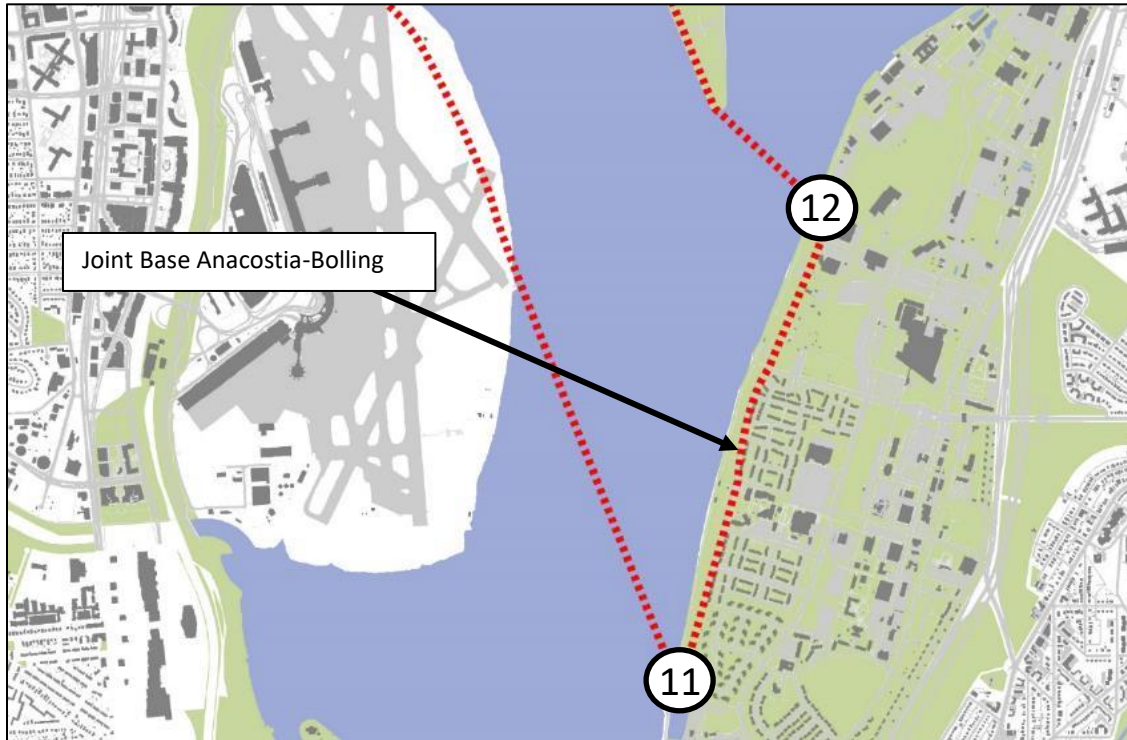
**Figure 2-17** | Photograph location 9. Long Bridge from north boundary of Reagan Airport at the Potomac River, facing north



**Figure 2-18** | Photograph location 10. Long Bridge from the southern edge of the airport, facing north/northwest



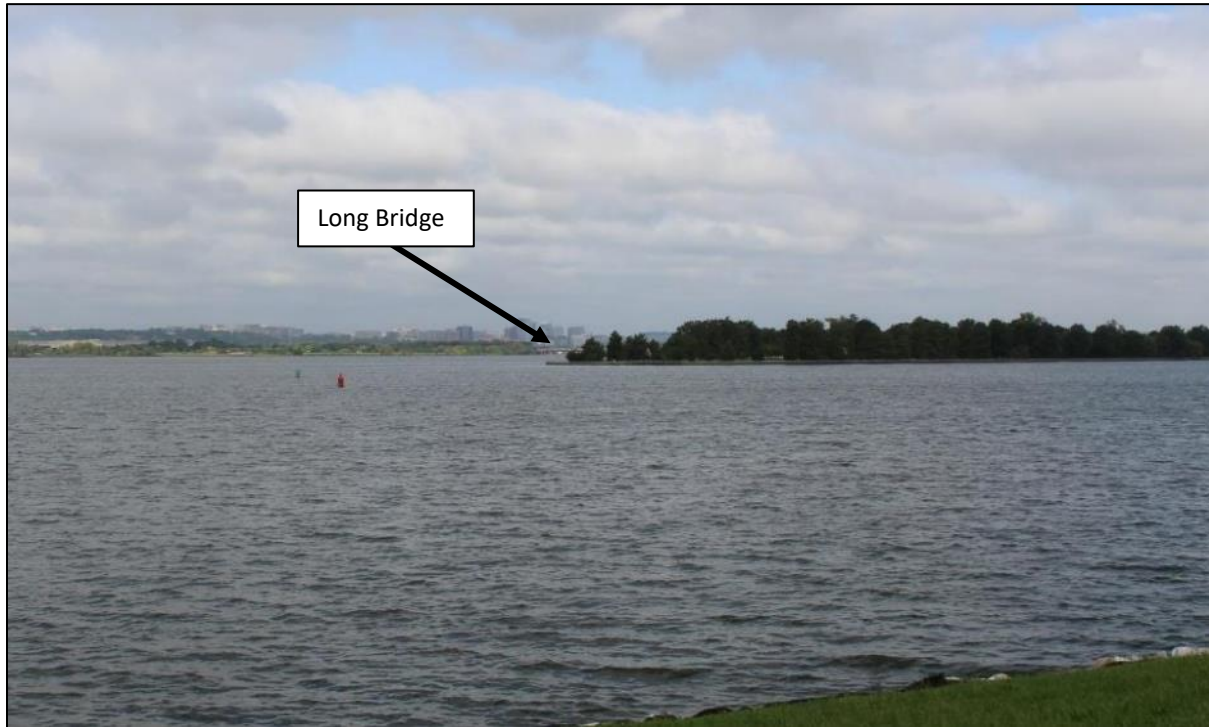
**Figure 2-19** | Map detail of photograph locations 11 and 12, Joint Base Anacostia-Bolling



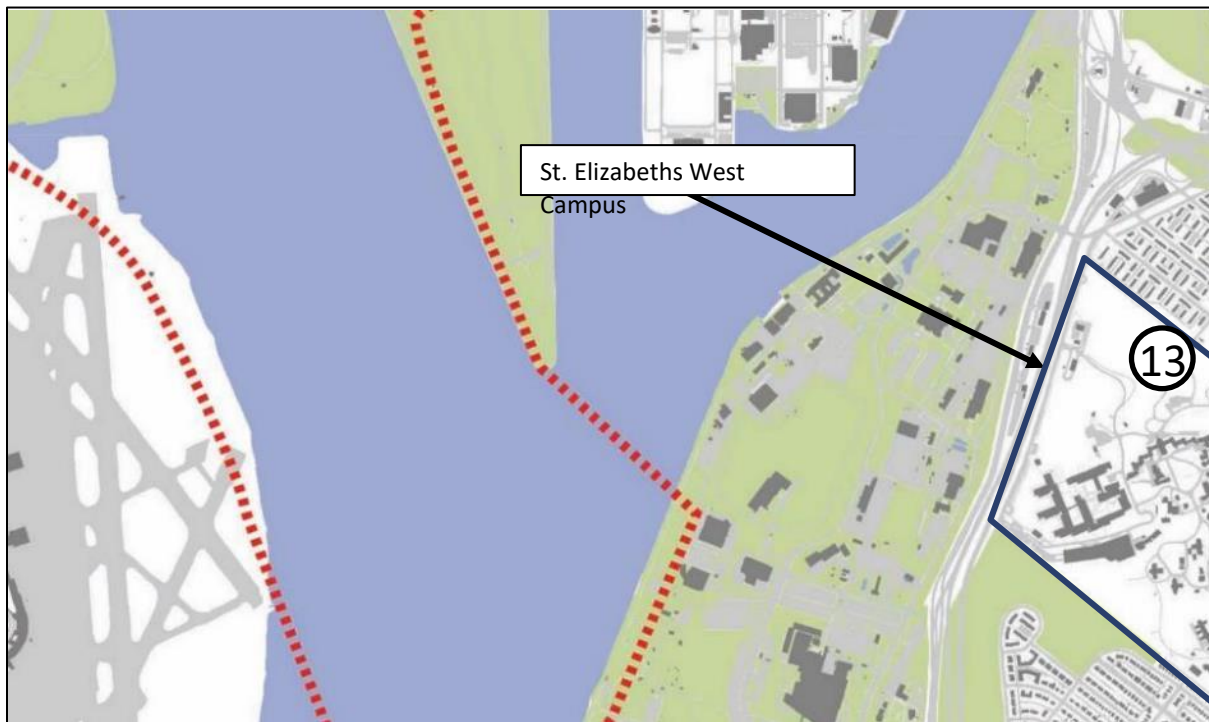
**Figure 2-20** | Photograph location 11. Long Bridge from Arnold Avenue, SW, facing northwest



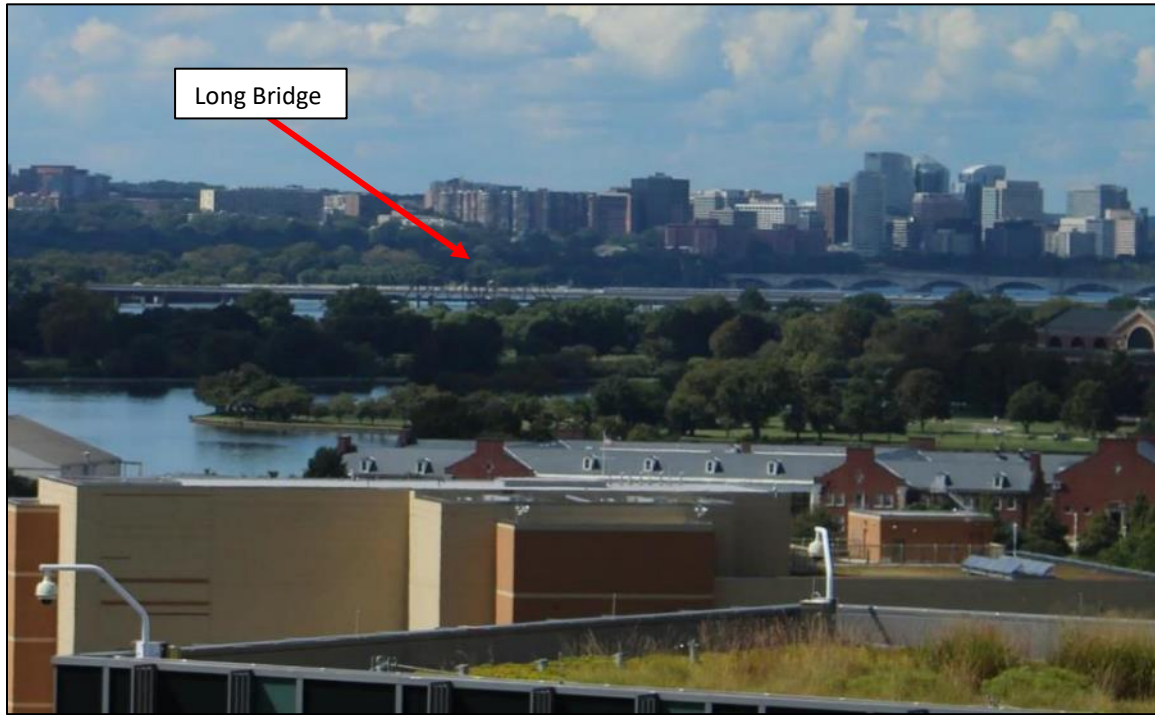
**Figure 2-21** | Photograph location 12. Long Bridge to the west of Boundary Drive at the Anacostia River, facing northwest



**Figure 2-22** | Map detail of photograph location 14, St. Elizabeths West Campus



**Figure 2-23** | Photograph 2. Long Bridge from Saint Elizabeths West Campus, facing northwest



**Figure 2-24** | Map detail of photograph locations 14, 15, and 16, East Potomac Park, Hains Point, and Fort McNair



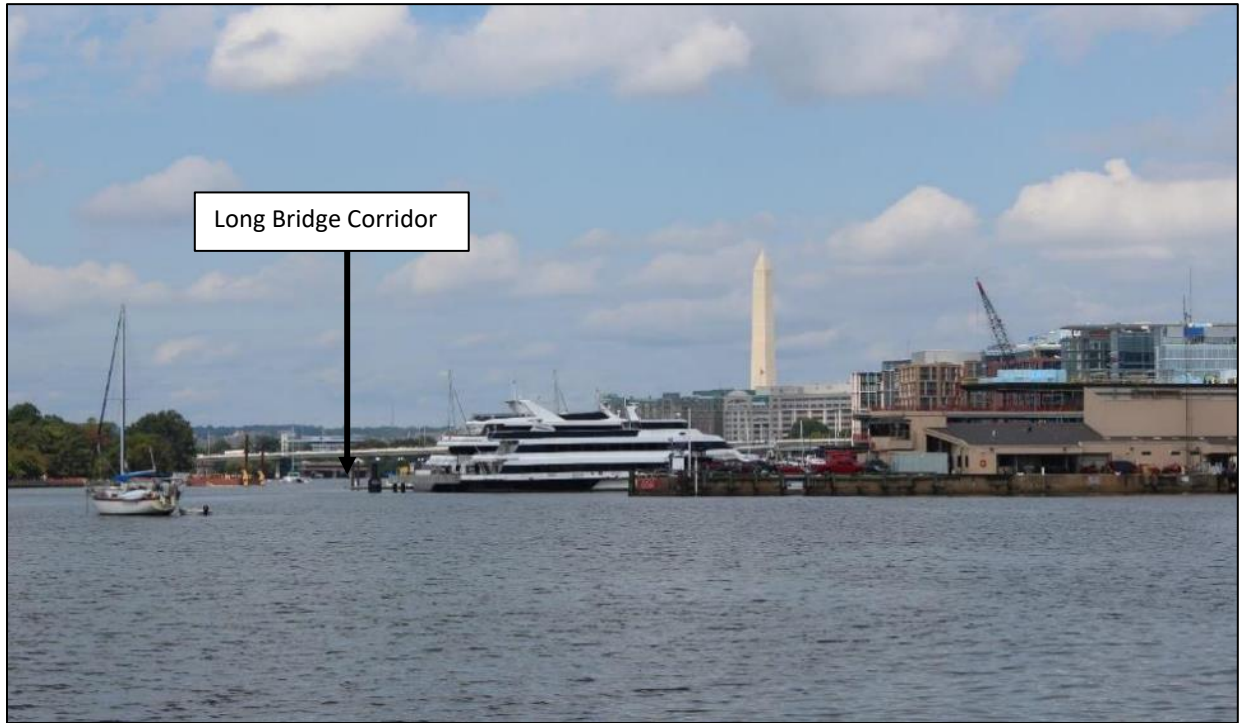
**Figure 2-25** | Photograph location 14. Long Bridge from Hains Point, facing northwest



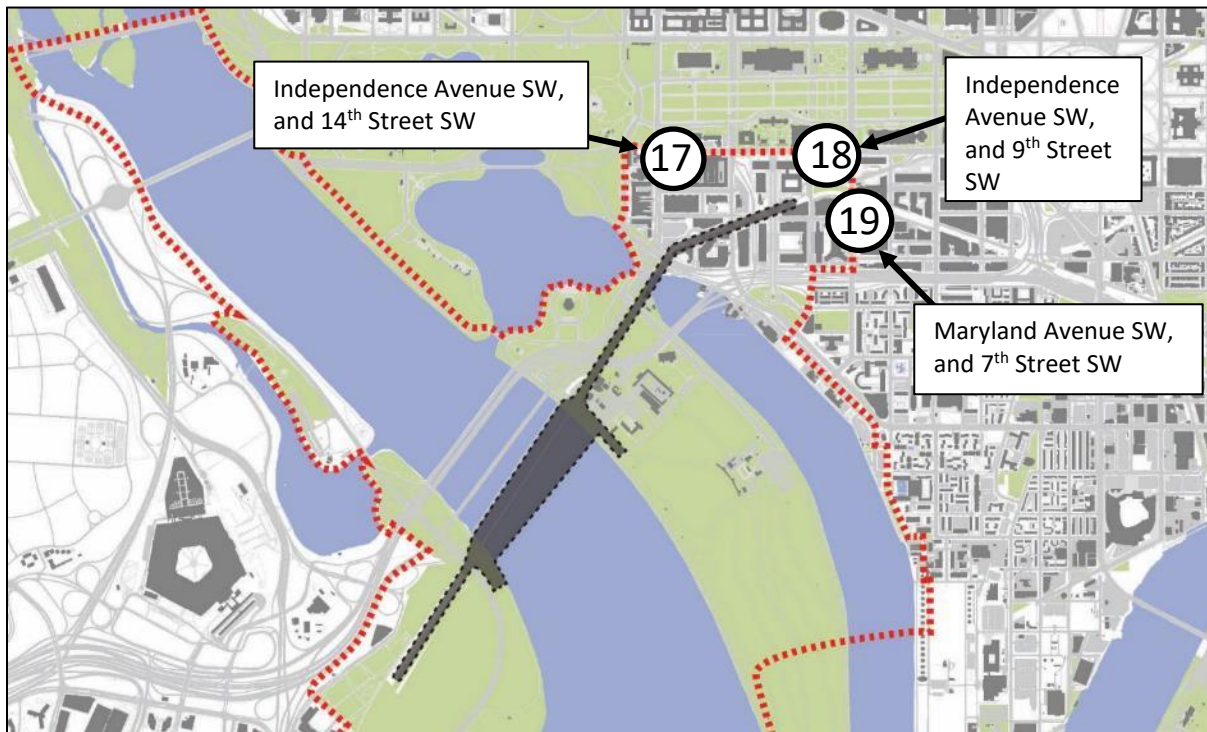
**Figure 2-26** | Photograph location 15. Long Bridge Corridor from East Potomac Park at the Washington Channel, facing northwest



**Figure 2-27** | Photograph location 16. Long Bridge Corridor from Fort McNair at B Street SW, facing northwest



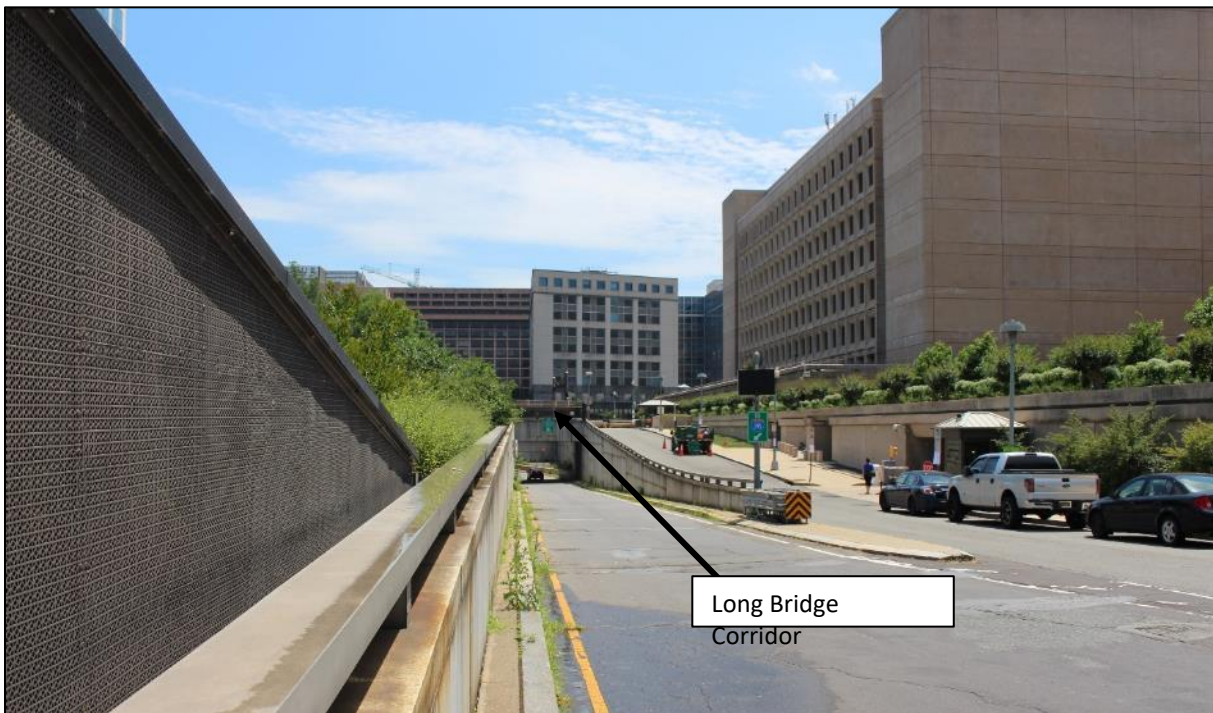
**Figure 2-28** | Map detail of photograph locations 17, 18, and 19



**Figure 2-29** | Photograph location 17. Long Bridge Corridor from Independence Avenue SW, and 14th Street SW facing south



**Figure 2-30** | Photograph location 18. Long Bridge Corridor from intersection of Independence Avenue SW and 9th Street SW, facing south





**Figure 2-31** | Photograph location 19. Long Bridge Corridor from intersection of Maryland Avenue SW, and 7th Street SW, facing southwest



### 3.0 Identification of Historic Properties

Once an APE has been defined, the Federal agency must “...make a reasonable and good faith effort...” to identify historic properties within its boundaries (36 CFR § 800.4(b)(1)). A historic property is defined as “any prehistoric or historic district, site, building, structure, or object included on, or eligible for inclusion in, the National Register of Historic Places (NRHP) maintained by the Secretary of the Interior. This term includes artifacts, records, and remains that are related to and located within such properties. The term includes properties of traditional religious and cultural importance to an Indian tribe or Native Hawaiian organization and that meet the National Register criteria” (including artifacts, records, and material remains relating to the district, site, building, structure or object” (36 CFR § 800.16(l)(1)).

In August 2016, FRA and DDOT completed the *Long Bridge Project, Environmental Data Collection Report* (Data Collection Report), which included preliminary identification of historic properties within and in the vicinity of the designated study area. The study area was defined by a 1,000-foot buffer along the length of the Long Bridge Corridor.<sup>9</sup> Historic properties were identified using the following information sources:

- Geographic Information System (GIS) mapping data provided by the District and Arlington County;
- DCSHPO Inventory of Historic Sites;
- NRHP database;
- General Services Administration (GSA) *Historic Buildings* website;
- Virginia Landmarks Register (VLR); and
- Virginia Cultural Resource Information System (V-CRIS).

The *Data Collection Report* was shared with several consulting parties, including VDHR and DCSHPO in September 2016, and the findings related to historic properties were again presented at the consulting party meetings in April and November 2017.

The APE has extended beyond this study area; as such, the above sources were reexamined to identify additional historic properties within the APE. The identification effort was expanded to include the following additional sources of information:

- Properties that are pending or have been recently listed in the NRHP, which were not listed in the August 2016 *Data Collection Report*;
- Properties that have been formally determined eligible for NRHP listing;
- Properties at or greater than 45 years of age that have not been previously evaluated for NRHP eligibility; and
- Contributing streets and avenues, views and vistas, reservations, and other contributing components listed in the Plan of the City of Washington (L’Enfant Plan; L’Enfant-McMillan Plan) NRHP Documentation.

In the future, the identification effort will be expanded to include:

- Potential archaeological resources within the Limits of Disturbance; and

---

<sup>9</sup> A 1000-foot buffer was uniformly selected for all environmental resources in the Data Collection Report. FRA selected this buffer to compile preliminary existing data on environmental resources within the vicinity of the Long Bridge Corridor; but it is not an indication that FRA has made any determination that effects would only occur within this 1000-foot buffer zone.

- Any additional feedback from DCSHPO, VDHR, and other consulting parties.

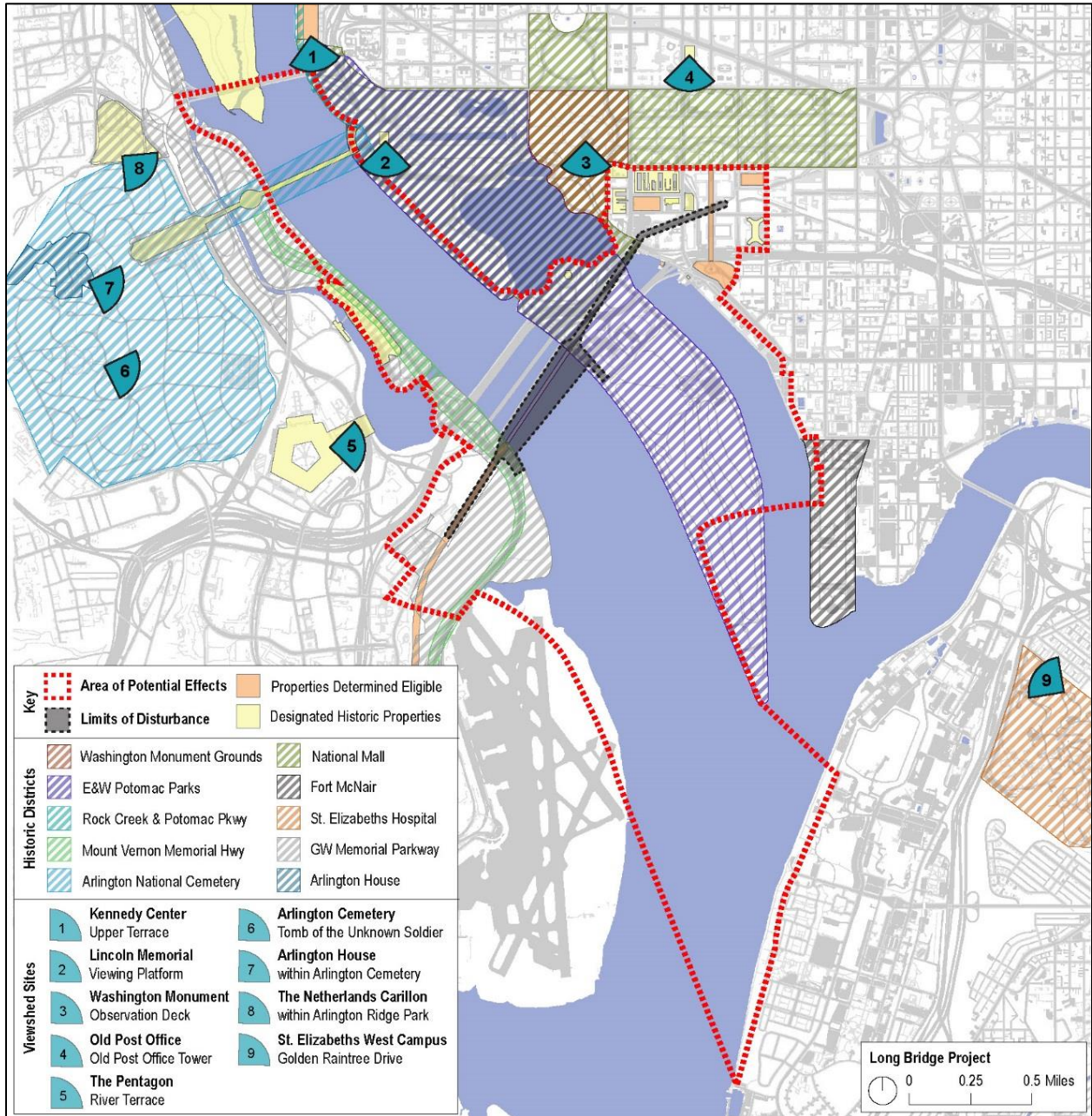
Although the scope for this project does not include drafting formal determinations of eligibility, properties located within the APE that are at least 45 years of age were evaluated against the NRHP Criteria for Evaluation.<sup>10</sup> An assessment of integrity for each property was also undertaken. This age was selected to account for the fifty-year threshold that is generally observed in the evaluation of historic significance, and to account for the implementation schedule of the Long Bridge Project (which may extend five or more years into the future). These properties were identified using a range of documentation resources including real property and building permit data, historic maps and photographs, and aerial photographs. A preliminary evaluation of each property's potential historic significance and integrity is provided herein as a resource for future, more detailed evaluation by the FRA or others at the time of project implementation.

Archaeological resources will be identified using a phased approach. FRA and DDOT will initiate the process by completing a Phase 1A Archaeological Assessment in consultation with DCSHPO and VDHR. The Phase 1A will consist of a desktop review of known archaeological sites and areas that exhibit high archaeological potential. The Phase 1A will address all alternatives, once a Preferred Alternative is identified, additional surveys will be conducted as needed. Because the U.S. Department of the Interior has jurisdiction over a majority of the area within the Limits of Disturbance (including the bottom lands of the Potomac River), FRA and DDOT will coordinate with the National Park Service regarding potential impacts to archaeological resources, including potential underwater archaeology.

---

<sup>10</sup> National Register of Historic Places, Frequently Asked Questions. <http://www.nationalregisterofhistoricplaces.com/faq.html>

**Figure 3-1 | Map of APE with Designated and Eligible Historic Properties**



### 3.1. Designated Historic Properties

The following properties have been listed in the NRHP, DC Inventory of Historic Sites (DC), and/or the VLR. Two properties have been designated as National Historic Landmarks (NHL). In some cases, these properties were determined eligible for National Register listing (Determination of Eligibility [DOE]) and were subsequently listed.

**Table 3-1** | List of historic properties previously listed in the NRHP, DC Inventory, or VLR. Several of the below properties listed on the DC Inventory have also been determined eligible for listing on the NRHP.

#	Name	Location	Designation
1.	National Mall Historic District	Washington, DC	DC, NRHP
2.	Parkways of the National Capital Region	Washington, DC	VLR, NRHP
3.	Rock Creek and Potomac Parkway Historic District	Along the Potomac River and Rock Creek from the Lincoln Memorial to the National Zoo.	DC, NRHP
4.	George Washington Memorial Parkway	Arlington County, (Extends to City of Alexandria and Fairfax County)	VLR, NRHP
5.	Mount Vernon Memorial Highway	Arlington County (Extends to City of Alexandria, and Fairfax County)	VLR, NRHP
6.	Plan of the City of Washington	Washington Region Multi-Property Submission	DC, NRHP
7.	East and West Potomac Parks Historic District	Washington, DC	DC, NRHP
8.	Thomas Jefferson Memorial	East Basin Drive SW, Washington, DC	DC, NRHP
9.	Central Heating Plant	325 13th Street SW, Washington, DC	DC, NRHP
10.	U.S. Department of Agriculture (USDA) Cotton Annex	300 12th Street SW, Washington, DC	DC, NRHP
11.	HUD Building (Robert C. Weaver Federal Building)	451 7th Street, SW, Washington, DC	DC, NRHP
12.	U.S. Department of Agriculture South Building	1352 C Street SW, Washington, DC	DC, NRHP
13.	Bureau of Engraving and Printing	301 14th Street SW, Washington, DC	DC
14.	Auditor's Building Complex	14th Street and Independence Avenue SW	DC, NRHP
15.	Arlington Memorial Bridge (and related features)	Memorial Avenue, DC & Virginia	DC, NRHP
16.	Fort Leslie J. McNair Historic District (The Old Arsenal)	4th and P Streets SW	DC, DOE
17.	Titanic Memorial	Water and P Streets SW	DC, NRHP
18.	Lunch Room Building and Oyster Shucking Shed	1100 Maine Avenue SW	DC, DOE
19.	Cuban Friendship Urn	Reservation 332, Ohio Drive at 14th Street Bridge SW	DC, NRHP
20.	Theodore Roosevelt Island National Memorial (Analoatan Island)	Potomac River west of Georgetown Channel	DC, NRHP
21.	Lyndon B. Johnson Memorial Grove	Columbia Island in Lady Bird Johnson Park	DC, NRHP
22.	Lincoln Memorial (Statue of Lincoln) *	West Potomac Park, Washington, DC	DC, NRHP

#	Name	Location	Designation
23.	Washington Monument and Grounds Historic District*	14th Street, between Constitution and Independence Avenues, Washington, DC	DC, NRHP
24.	Arlington House Historic District*	Roughly bound by Sheridan Drive, Ord and Weitzel Drive, Humphrey's Drive and Lee Avenue in Arlington National Cemetery	VLR, NRHP
25.	Arlington National Cemetery Historic District*	One Memorial Avenue, Arlington, VA	NRHP
26.	St. Elizabeths Hospital Historic District*	2700 Martin Luther King Jr., Avenue, SE	DC, NRHP, NHL
27.	Netherlands Carillon (within Arlington Ridge Park)*	Northwest corner of N Meade Street and Marshall Drive in Arlington, VA	VLR, NRHP,
28.	Old Post Office*	1100 Pennsylvania Avenue, NW	DC, NRHP
29.	The Pentagon*	U.S. 1, Va. 110, and Interstate 395	VLR, NRHP, NHL

\* These properties are designated as viewshed locations outside of the contiguous APE boundaries.

### 1. National Mall Historic District

*Location:* Washington, DC

*Designation:* DC, NRHP

The National Mall Historic District (the Mall) is comprised of the monumental core of Washington, DC, an original design element of Major General Pierre Charles L'Enfant's Plan for the Capital City. The L'Enfant Plan was further refined and expanded in the McMillan Commission's 1901-1902 plan for the City of Washington. L'Enfant designed the National Mall to serve as the central axis of Washington's monumental core. The Plan called for the Mall to be a 400-foot-wide, mile long, "grand avenue" from the Capitol to a point directly south of the President's house. The site was to be lined with landscaped areas and gardens. The 1901 McMillan Commission restored and supplemented the L'Enfant Plan primarily by removing obtrusive elements and bordering the Mall with public buildings.

Figure 3-2 | National Mall



## 2. Parkway of the National Capital Region

*Location:* Throughout the Washington, DC, metropolitan region.

*Designation:* NRHP, VLR

Multi-property documentation for scenic parkways of the Washington, DC region including the George Washington Memorial Parkway, the Mount Vernon Memorial Highway, and the Rock Creek and Potomac Parkway, among others.

**Figure 3-3 |** Rock Creek and Potomac Parkway



## 3. Rock Creek and Potomac Parkway

*Location:* Along the Potomac River and Rock Creek from the Lincoln Memorial to the National Zoo.

*Designation:* DC, NRHP

The first parkway for which legislation was passed in the Nation's Capital and one of the earliest parkways constructed in the region. In 1913, Congress passed the Public Buildings Act, which authorized the creation of the parkway. Planning, design, and land acquisition of the parkway continued through the 1930s, and the parkway was completed in 1935.

**Figure 3-4 |** Rock Creek and Potomac Parkway



## 4. George Washington Memorial Parkway

*Location:* Arlington County, City of Alexandria, and Fairfax County

*Designation:* VLR, NRHP

The George Washington Memorial Parkway is a 25-mile scenic parkway administered by the National Park Service. Constructed predominantly in the 1930s, the parkway provides a ceremonial and recreational corridor between northern Virginia and Mount Vernon, the home and estate of George Washington.

**Figure 3-5 |** George Washington Memorial Parkway (Mount Vernon)



### 5. Mount Vernon Memorial Highway

*Location:* Arlington County, City of Alexandria, and Fairfax County

*Designation:* VLR, NRHP

Original 15.2-mile segment of the scenic parkway commemorating the birth of George Washington.

**Figure 3-6 |** Mount Vernon Memorial Highway (Google Maps)



### 6. Plan of the City of Washington

*Location:* Includes original elements of Pierre Charles L'Enfant's plan for the City of Washington, including later elements proposed by the McMillan Commission

*Designation:* NRHP, DC

Multi-property submission for the street grid, diagonal avenues, parks, vistas among monuments and sites over Federal land within the L'Enfant Plan boundary, and the airspace above this matrix up to the legal height limit in the City.

**Figure 3-7 |** Detail, L'Enfant Plan Facsimile, 1887 (Library of Congress)



### 7. East and West Potomac Parks Historic District

*Location:* Washington, DC

*Designation:* NRHP, DC

Historic district comprising 730 acres of park land along the Potomac River. Standing memorials in the parks include the Lincoln and Jefferson Memorials. Contributing features to this historic district include the Inlet Bridge, the U. S. Engineers Storehouse, the National Capital Region Building complex, East Potomac Park Golf Course, East Potomac Park Field House, East Potomac Park Swimming Pool, and D-1 Substation Building.

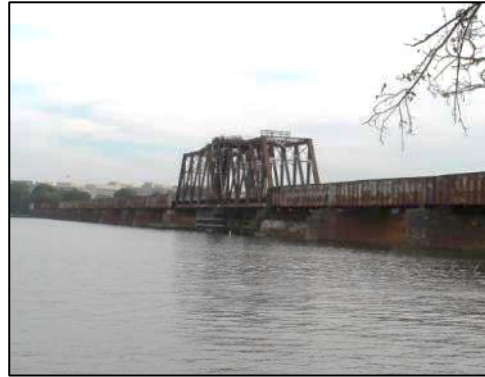
**Figure 3-8 |** Hains Point, East and West Potomac Parks Historic District





The Long Bridge, constructed in 1904, is a contributing feature to the East and West Potomac Parks historic district.<sup>11</sup>

**Figure 3-9 |** Long Bridge



**8. Thomas Jefferson Memorial**

*Location:* 16 East Basin Drive SW, Washington, DC  
*Designation:* NRHP, DC

**Figure 3-10 |** Jefferson Memorial (National Park Service)

National Memorial dedicated to third U.S. President Thomas Jefferson. Designed by notable architect John Russell Pope, the memorial was constructed between 1937 and 1942. Sited facing the Tidal Basin, the memorial forms a significant component of the city’s monumental plan.



**9. Central Heating Plant**

*Location:* 325 13th Street SW, Washington, DC  
*Designation:* NRHP, DC

**Figure 3-11 |** Central Heating Plant

A heating plant completed in 1934 to supply steam to Federal buildings. Designed in the Art Deco style by architect Paul Phillippe Cret under the direction of the Supervising Architect of the Treasury Department.



<sup>11</sup> The Evening Star. 1904. *First Train Passes, New Railway Bridge Used for First Time*. August 25, 1904.

**10. USDA Cotton Annex**

*Location:* 300 13th Street SW, Washington, DC  
*Designation:* NRHP, DC

The Bureau of Agricultural Economics (BAE) Building, now known as the Cotton Annex, was built in 1936 to 1937 for the USDA under the auspices of Supervising Architect of the Treasury Louis A. Simon (1933–1939).

**Figure 3-12 |** USDA Cotton Annex



**11. U.S. Department of Housing and Urban Development (HUD) Building (Robert C. Weaver Federal Building)**

*Location:* 451 7th Street SW, Washington, DC  
*Designation:* NRHP, DC

Completed in 1968 by the architect Marcel Breuer. The modernist design and execution of the HUD building exemplifies the primary tenets of the "Guiding Principles for Federal Architecture" as set forth by President John F. Kennedy's administration in 1962.

**Figure 3-13 |** HUD Building



**12. USDA South Building**

*Location:* 1352 C Street SW, Washington, DC  
*Designation:* DC, NRHP

Completed in 1936, the South Building is significant for its association with the growth of the Department of Agriculture; broader patterns of city development in the District; and as an excellent example of the Stripped Classical style of Federal architecture of the 1930s.

**Figure 3-14 |** USDA South Building



**13. Bureau of Engraving and Printing (BEP) (Main Building)**

*Location:* 301 14th Street SW, Washington, DC

*Designation:* DC

The building was designed by the Office of the Supervising Architect of the Treasury, under Supervising Architect James Knox Taylor. The Neoclassical style building was completed in February 1914.

**Figure 3-15 |** BEP Main Building



**14. Auditor's Building Complex**

*Location:* 14th Street and Independence Avenue SW, Washington, DC

*Designation:* DC, NRHP

The Auditors Building was the first building designed and constructed for the U.S. Department of the Treasury Bureau of Engraving and Printing. Originally completed in 1880, the building had three major additions in 1891, 1895, and 1900. Originally designed by James B. Hill, Supervising Architect of the Treasury Department, the building is also significant for its architectural style.

**Figure 3-16 |** Auditor's Building (Library of Congress)



**15. Arlington Memorial Bridge (and Related Features)**

*Location:* Memorial Avenue, DC and Virginia

*Designation:* DC, NRHP

The 1932 bridge and its related features are a major element of 1902 McMillan Commission plan for the city. The bridge serves as a symbolic link between the north and the south, connecting Arlington House (home of Robert E. Lee) and the Lincoln Memorial.

**Figure 3-17 |** Memorial Bridge



**16. Fort Leslie J. McNair Historic District (The Old Arsenal)**

*Location:* Fourth and P Streets SW, Washington, DC

*Designation:* DC, DOE

Fort McNair was established in 1791 and today is the third oldest U.S. Army installation in continuous use. The district is significant in the fields of architecture, military history, military education, and health and medicine.

**Figure 3-18 |** Fort McNair (National Defense University)



**17. Titanic Memorial**

*Location:* Water and P Streets SW, Washington, DC

*Designation:* DC, NRHP

The Titanic Memorial was designed by the female sculptor Gertrude Vanderbilt Whitney. The sculpture is significant as it is only one of five located in the District designed by a woman. Completed in 1916, the statue was originally erected at the Rock Creek and Potomac Parkway in 1930. In 1968, the statue was relocated to its present location.

**Figure 3-19 |** Titanic Memorial



**18. Lunch Room Building and Oyster Shucking Shed**

*Location:* 1100 Maine Avenue SW, Washington, DC

*Designation:* DC, DOE

The Lunch Room Building and Oyster Shucking Shed are significant as they are the only extant buildings associated with the 1916-1918 Municipal Fish Wharf and Market on Water Street. The buildings illustrate Congress' support for the City Beautiful movement as implemented by the improvement of the District's shoreline, and recognition of the need to address issues with the District's fishing industry, as well as they health and welfare of the District's citizens.

**Figure 3-20 |** Lunch Room



**19. Cuban Friendship Urn**

*Location:* Reservation 332, Ohio Drive at Fourteenth Street Bridge SW, Washington, DC

*Designation:* DC, NRHP

The urn is significant as it is the second gift of sculpture presented to the District of Columbia by a foreign nation. It was presented to President Calvin Coolidge in Havana in 1928, and Congress authorized its acceptance on May 22, 1928.

**Figure 3-21 |** Cuban Friendship Urn (Wikimedia Commons)



**20. Theodore Roosevelt Island National Memorial (Analoostan Island)**

*Location:* Potomac River west of Georgetown Channel

*Designation:* DC, NRHP

The 88-acre island is a memorial to Theodore Roosevelt, twenty-sixth President of the United States. It was presented to the U.S. by the Roosevelt Memorial Association in 1931 and opened to the public in 1936.

**Figure 3-22 |** Roosevelt Memorial (National Park Service)



**21. Lyndon B. Johnson Memorial Grove on the Potomac**

*Location:* George Washington Memorial Parkway

*Designation:* NRHP

Authorized by Congress in 1973, the Memorial Grove established an official memorial to President Lyndon B. Johnson. The site is significant for its association with the historic pattern of creating presidential memorials, which began with the Washington Monument, and as an excellent example of twentieth century landscape architecture.

**Figure 3-23** | Johnson Memorial Grove (National Park Service)



**22. Lincoln Memorial (Statue of Lincoln)**

*Location:* West Potomac Park, Washington, DC

*Designation:* DC, NRHP

The Lincoln Memorial is significant as an important example of Neoclassical style architecture. It is the foremost memorial to the sixteenth President of the United States, and as the terminus of the extended Mall plan in the Senate Park Commission's (popularly known as the McMillan Commission) 1902 plan for the city. The memorial was designed by architect Henry Bacon, and Lincoln's statue is the work of sculptor Daniel Chester French.

**Figure 3-24** | Lincoln Memorial (National Park Service)



### 23. Washington Monument and Grounds Historic District

*Location:* 14th Street, between Constitution and Independence Avenues, Washington, DC

*Designation:* DC, NRHP

The Washington Monument and Grounds Historic District is significant under Criterion A in the areas of: politics and government as part of the establishment of the national capital; social history as a gathering place for the American citizenry to express their First Amendment rights; ethnic heritage for its association with the 1963 March on Washington for Jobs and Freedom; and locally as the site of continuing entertainment and recreation. The historic district is also significant for its architecture, planning, and design, and as a planned cultural landscape. There are several views and vistas that contribute to the significance of the historic district, including views from the top of the monument to surrounding city and important sites.

**Figure 3-25** | Washington Monument and Grounds (National Park Service)



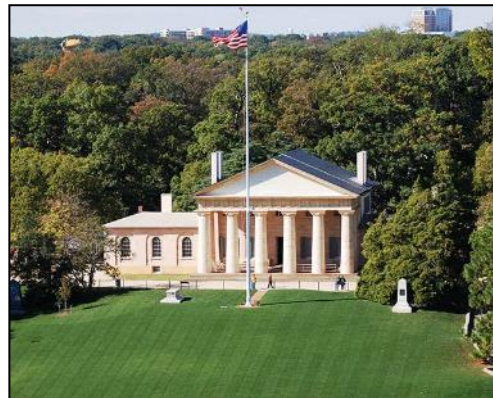
### 24. Arlington House Historic District

*Location:* Roughly bound by Sheridan Drive, Ord and Weitzel Drive, Humphrey's Drive and Lee Avenue in Arlington National Cemetery, Arlington, VA

*Designation:* VLR, NRHP

The Arlington House Historic District is significant for its association with George Washington Parke Custis (step-grandson of George Washington) and General Robert Edward Lee (military leader and important figure in the American Civil War); its architecture and landscape design; its reflection of the ethnic heritage of enslaved African Americans and household slaves who worked and lived on site; its association with Arlington National Cemetery; as one of the Federal government's first attempts at historic preservation (1925 legislation, 1928-1935 restoration); and its archaeology. There are several views and vistas that contribute to the significance of the historic district, including views from the house eastward. Arlington House Historic District is located within the boundaries of the Arlington National Cemetery Historic District. It

**Figure 3-26** | Arlington House (National Park Service)



is not administered by Arlington Cemetery but rather separately by the National Park Service.

**25. Arlington National Cemetery Historic District**

*Location:* One Memorial Avenue, Arlington, VA

*Designation:* NRHP

Arlington National Cemetery Historic District is significant as the country's most sacred national cemetery. Created from the former estate of Mary Anna Custis Lee (wife of Civil War Confederate General Robert E. Lee) and purchased by the Federal Government in 1864, the site includes several significant contributing architectural features, including Arlington House, the Tomb of the Unknown Soldier, the Arlington Memorial Amphitheater, and numerous additional memorials. The current Long Bridge is visible from Arlington House, the Tomb of the Unknown Soldier, and their immediately surrounding landscapes.

**Figure 3-27 |** Arlington National Cemetery (Arlington Cemetery)



**26. St. Elizabeths Hospital Historic District**

*Location:* 2700 Martin Luther King Jr., Avenue SE, Washington, DC

*Designation:* DC, NRHP, NHL

St. Elizabeths Hospital Historic District is one of the nation's earliest institutions for the treatment of mental illness. Established through the efforts of Dorothea Dix, the leading mental health reformer of the 19th century, the hospital was chartered by Congress in 1852 as the Government Hospital for the Insane, with the

**Figure 3-28 |** St. Elizabeths West Campus



mission to provide humane care for patients from the Army, Navy, and District of Columbia. The historic district features a significant collection of late-19th and early 20th-century architecture, including the Center Building (1853-1855), an early example of the linear plan for mental hospital wards developed by reformer Thomas Kirkbride.



**27. Netherland Carillon (within Arlington Ridge Park)**

*Location:* Within Arlington Ridge Park at the northwest corner of N Meade Street and Marshall Drive in Arlington, VA

*Designation:* Contributing resource within Arlington Ridge Park (NRHP, VLR)

The Netherlands Carillon is located at the south end of Arlington Ridge Park. The Netherlands Carillon, designed by Dutch architect Joost W.C. Boks, is a Modernist steel framework with a memorial carillon. The carillon was presented as a gift to the United States by the Netherlands in thanks for the aid provided by the United States during and after World War II. The carillon is set within a picturesque landscape designed by National Park Service landscape architects in the early 1960s. The Netherlands Carillon appears to be potentially individually eligible per NPS documentation.

**Figure 3-29 |** The Netherlands Carillon (National Park Service)



**28. Old Post Office**

*Location:* 1100 Pennsylvania Avenue, NW

*Designation:* DC, NHRP (located within Federal Triangle (DC, DOE) and Pennsylvania Avenue National Historic Site (NHS, NR, DC)

The Old Post Office and Clock Tower (1891 – 1899) was designed by the Office of the Supervising Architect of the Treasury under Willoughby J. Edbrooke to house both the Post Office Department as well as the City Post Office. The first Federal Office building to be constructed in the area later known as Federal Triangle, it is one of the few Romanesque Revival style buildings of monumental scale to be constructed in Washington. At the time of its completion, its 315-foot clock tower was the third highest building in the District, after the Washington Monument and the Capitol.

**Figure 3-30 |** The Old Post Office (National Park Service)



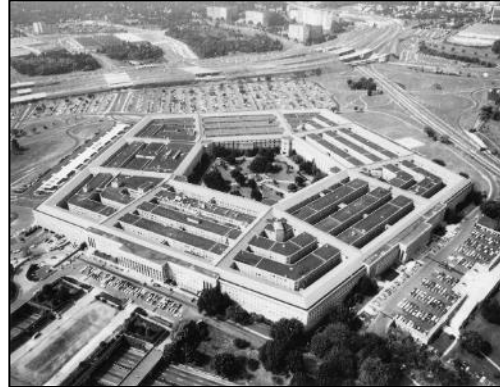
## 29. The Pentagon

*Location:* U.S. 1, Va. 110, and Interstate 395

*Designation:* VLR, NRHP, NHL

The Pentagon (1941 – 1943) was primarily designed by architects George Edwin Bergstrom and David J. Witmer. The Pentagon is significant as a NHL for its association with “events that have made a significant contribution to the geopolitical role of the United States as a world power” from World War II to the present, and for its association with the lives of nationally significant individuals from 1941 to today. Although the building’s architecture did not qualify the building as an NHL, the building is considered architecturally important as it embodies the Stripped Classical style of architecture popular during the period, and as the largest and one of the last of Washington’s monumental buildings designed in accordance with the McMillan Commission’s 1902 plan for the City of Washington.

**Figure 3-31** | The Pentagon (VDHR)



### 3.2. Eligible Historic Properties

The following properties have been determined eligible or recommended as eligible for listing in the National Register of Historic Places.

**Table 3-2** | List of historic properties that have been determined eligible for listing in the NRHP by a Federal agency or recommended as eligible by a SHPO.

#	Name	Location	Designation
1.	<b>Bureau of Engraving and Printing Annex</b>	300 14th Street SW, Washington, DC	DOE
2.	<b>Federal Office Building 10A (Orville Wright Building)</b>	800 Independence Ave SW, Washington, DC	DOE
3.	<b>Benjamin Banneker Park/Overlook; Tenth Street Overlook</b>	Terminus of 10th Street SW, Washington, DC	DOE
4.	<b>Richmond, Fredericksburg and Potomac Railroad Historic District</b>	Along CSX right-of-way in VA from Arlington County to the City of Richmond	DOE
5.	<b>Washington Marina Building</b>	1300 Maine Avenue SW	DOE
6.	<b>L'Enfant Promenade</b>	Section of 10th Street SW between Independence Avenue and Banneker Park	DOE
7.	<b>Lady Bird Johnson Park</b>	George Washington Memorial Parkway	DOE
8.	<b>John F. Kennedy Center for the Performing Arts*</b>	2700 F Street NW, Washington, DC	DOE
9.	<b>Liberty Loan Federal Building</b>	401 14th Street SW, Washington, DC	DOE

*\* These properties are designated as viewshed locations outside of the contiguous APE boundaries.*

#### 1. Bureau of Engraving and Printing (BEP) Annex

*Location:* 300 14th Street SW, Washington, DC

*Designation:* DOE

The BEP Annex was constructed between 1936-1938 for the BEP under the auspices of the Office of the Supervising Architect, Louis A. Simon, Supervising Architect, and Neal A. Melick, Supervising Engineer. The BEP Annex is significant for its association with the operation and growth of the BEP during the twentieth century, and as a distinctive example of a Stripped Classic style Federal building constructed in the 1930s.

**Figure 3-32** | BEP Annex



**2. FOB 10A; Orville Wright Building**

*Location:* 800 Independence Avenue SW, Washington, DC

*Designation:* DOE

FOB 10A was originally constructed between 1961 and 1963 for GSA, and was one of the earliest to be constructed as part of the urban renewal program for southwest Washington, DC. The International style building was designed by the architectural firms of Holabird & Root & Burgee, and Carroll, Grisdale & Van Alen.

**Figure 3-33 | FOB 10A (GSA)**



**3. Benjamin Banneker Park/Overlook; Tenth Street Overlook**

*Location:* Terminus of 10th Street SW, Washington, DC

*Designation:* DOE

Landscape completed in 1969 and designed by landscape architect Dan Kiley, is a 200-foot wide elliptical concrete plaza with a large, central, conical, fountain of green granite. Designed and constructed as part of the National Capital Planning Commission’s (NCPC) 1956 Urban Renewal Plan: Southwest Urban Renewal Project C.

**Figure 3-34 | Banneker Park**



**4. Richmond, Fredericksburg and Potomac Railroad Historic District**

*Location:* Along CSX right-of-way in eastern Virginia from the Potomac River in Arlington County to the South Broad Street Station in the City of Richmond, VA

*Designation:* DOE (recommended as eligible by VDHR staff)

The Richmond, Fredericksburg, and Potomac Railroad was a railroad connecting Richmond, Virginia, to Washington, DC. The railroad corridor conveys its association with transportation from ca. 1837 through 1943, when the demand for railroad transportation began to wane. In 2017, VDHR staff recommended the railroad corridor potentially eligible as an historic district.

**Figure 3-35 | Richmond, Fredericksburg and Potomac Railroad (Richmond, Fredericksburg & Potomac Railroad Historical Society, Inc.)**



### 5. Washington Marina Building

*Location:* 1300 Maine Avenue SW, Washington, DC

*Designation:* DOE

Completed in 1938, the Washington Marina Building was an element of a larger Works Progress Administration (WPA) project to improve the Washington Channel. The project was completed by the WPA and the U.S. Army Corps of Engineers. The building is significant for its association with the WPA and improvement of the District's waterfront.

**Figure 3-36 |** Washington Marina Building



### 6. L'Enfant Promenade

*Location:* Section Tenth Street SW between Independence Avenue and Banneker Park

*Designation:* DOE

The promenade, originally known as the Tenth Street Mall, was a key element of I.M Pei and Harry Weese's plan for Southwest Redevelopment Area. The promenade is significant for its association with the creation and implementation of the NCPC's 1950 *Comprehensive Plan for the District of Columbia*.

**Figure 3-37 |** L'Enfant Promenade



### 7. Lady Bird Johnson Park

*Location:* In the George Washington Memorial Parkway along the Potomac River, directly across the river from West Potomac Park

*Designation:* DOE

The park is comprised of a man-made island, originally known as Columbia Island, that was constructed between 1915 and 1930. The park was constructed in connection with the Arlington Memorial Bridge's construction. In the 1960s and 1970s, the island was improved as part of the Johnson Administration's beautification program, and by a tree planting plan

**Figure 3-38 |** Lady Bird Johnson Park (Cultural Landscape Foundation)



designed by the landscape architect Edward Durrell Stone, Jr.

### 8. John F. Kennedy Center for the Performing Arts

*Location:* 2700 F Street NW, Washington, DC

*Designation:* DOE

The Modernist style building was designed by the American architect Edward Durrell Stone and was constructed between 1964 and 1971. The Kennedy Center has been determined historically significant as an important work by Stone, and as the only memorial to President Kennedy in the vicinity of Washington, DC.

**Figure 3-39** | Kennedy Center

(Wikimedia Commons)



### 9. Liberty Loan Federal Building

*Location:* 401 14th Street SW, Washington, DC

*Designation:* DOE

The building was originally constructed as one of many temporary office buildings to support wartime bureaucratic expansion and housed the Liberty Loans bond program during World War I. It is the only surviving “tempo” building. The building has housed several Treasury organizations and Federal agencies. Today, the building is used by the U.S. Department of the Treasury’s Bureau of the Fiscal Service.<sup>12</sup> DCSHPO and the General Services Administration (GSA) consider the building eligible for NRHP listing and GSA is currently preparing a formal DOE.

**Figure 3-40** | Liberty Loan Federal

Building (Google Maps)



<sup>12</sup> “Liberty Loan Federal Building,” GSA, accessed October 18, 2017, <https://www.gsa.gov/real-estate/gsa-properties/visiting-public-buildings/liberty-loan-federal-building>.

### 3.3. Properties at or Greater than Forty-Five Years of Age

The following properties were constructed prior to 1972. Preliminary determinations have been made regarding each property's potential eligibility for listing in the NRHP.

**Table 3-3** | List of historic properties that have been determined eligible for listing in the NRHP by a Federal agency or recommended as eligible by a SHPO.

#	Name	Location	Date(s)	Preliminary Determination of Eligibility
1.	<b>425 12<sup>th</sup> Street SW</b>	425 12 <sup>th</sup> Street SW, Washington, DC	1959	Likely not eligible.
2.	<b>Astral Building (North Building, L'Enfant Plaza)</b>	955 L'Enfant Plaza, SW Washington, DC	1968	Potentially eligible.
3.	<b>Comsat Building (South Building, L'Enfant Plaza)</b>	950 L'Enfant Plaza, SW Washington, DC	1965	Potentially eligible.
4.	<b>Loew's L'Enfant Plaza Hotel (East Building, L'Enfant Plaza)</b>	470-490 L'Enfant Plaza SW, Washington, DC	1971 to 1973	Potentially eligible.
5.	<b>USPS Building (West Building, L'Enfant Plaza)</b>	475 L'Enfant Plaza, SW Washington, DC	1969 to 1971	Potentially eligible.
6.	<b>398 Long Bridge Drive</b>	398 Long Bridge Drive, Arlington, VA	1957	Likely not eligible.

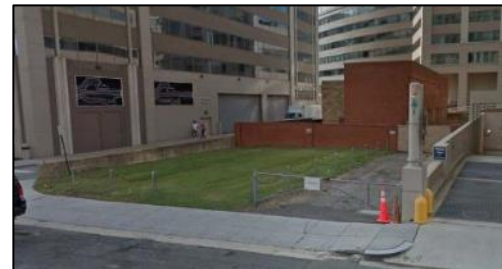
#### 1. 425 12<sup>th</sup> Street, SW

*Location:* 425 12th Street SW, Washington, DC

*Date of Construction:* 1959

A one-story brick substation surrounded by a solid brick fence owned by PEPCO. Although the nondescript utilitarian building appears to maintain its integrity, based on cursory research it does not appear to meet the National Register criteria for evaluation. As such, the property is likely not eligible for listing in the NRHP.

**Figure 3-41** | 425 12th Street, SW (Google Maps)



## 2. Astral Building (North Building, L'Enfant Plaza)

*Location:* 955 L'Enfant Plaza SW, Washington, DC

*Date of Construction:* 1968

Designed by Araldo A. Cossutta, a partner with the architectural firm of I.M. Pei and Partners. Completed as part of Phase I of L'Enfant Plaza. The building is part of the larger L'Enfant Plaza complex, which includes the Comsat Building (South Building) (1965), Loew's L'Enfant Plaza Hotel (East Building) (1971 to 1973), USPS Building (West Building) (1969 to 1971) and the plaza.<sup>13</sup> L'Enfant Plaza was a major feature of the urban renewal of the southwest quadrant of Washington, DC, that took place during the mid-20<sup>th</sup> century, and is an interesting example of the Brutalist style in Washington, DC. The building appears to maintain sufficient integrity of location, design, setting, materials, workmanship, feeling and association to convey its significance. As such, the property is potentially eligible for listing in the NRHP.

**Figure 3-42** | Astral Building (Google Maps)



## 3. Comsat Building (South Building, L'Enfant Plaza)

*Location:* 955 L'Enfant Plaza SW, Washington, DC

*Date of Construction:* 1965

Designed by Araldo A. Cossutta, a partner with the architectural firm of I.M. Pei and Partners. Completed as part of Phase I of L'Enfant Plaza. The building is part of the larger L'Enfant Plaza complex, which includes the Astral Building (North Building) (1968), Loew's L'Enfant Plaza Hotel (East Building) (1971 to 1973), USPS Building (West Building) (1969 to 1971) and the plaza.<sup>14</sup> L'Enfant Plaza was a major feature of the urban renewal of the southwest quadrant of Washington, DC, that took place during the mid-20<sup>th</sup> century, and is an interesting example of the Brutalist style in Washington, DC. The building appears to maintain sufficient integrity of location, design, setting, materials, workmanship, feeling and association to convey its significance. As such, the property is potentially eligible for listing in the NRHP.

**Figure 3-43** | Comsat Building (Google Maps)



<sup>13</sup> Francesca Russello Ammon, *Historic American Buildings Survey, Southwest Washington Urban Renewal Area, HABS DC-856* (2004), 91.

<sup>14</sup> Francesca Russello Ammon, *Historic American Buildings Survey, Southwest Washington Urban Renewal Area, HABS DC-856* (2004), 91.



#### 4. Loew's L'Enfant Plaza Hotel (East Building, L'Enfant Plaza)

*Location:* 470-490 L'Enfant Plaza SW, Washington, DC

*Date of Construction:* 1971 to 1973

Part of the second phase of the L'Enfant Plaza construction. Construction of the building began in fiscal year 1971 and was completed in 1973. The building was designed by Vlasimil Koubek, a local architect. The building is part of the larger L'Enfant Plaza complex, which includes the Astral Building (North Building) (1968), Comsat Building (South Building) (1965), USPS Building (West Building) (1969 to 1971) and the plaza.<sup>15</sup> L'Enfant Plaza was a major feature of the urban renewal of the southwest quadrant of Washington, DC, that took place during the mid-20<sup>th</sup> century, and is an interesting example of the Brutalist style in Washington, DC. The building appears to maintain sufficient integrity of location, design, setting, materials, workmanship, feeling and association to convey its significance. As such, the property is potentially eligible for listing in the NRHP.

**Figure 3-44** | Loew's L'Enfant Plaza Hotel (Google Maps)



#### 5. USPS Building (West Building, L'Enfant Plaza)

*Location:* 475 L'Enfant Plaza SW, Washington, DC

*Date of Construction:* 1969 to 1971

Part of the second phase of the L'Enfant Plaza construction, the building was separated from the plaza by the L'Enfant Promenade. Construction of the building began in 1969 and the building was completed in 1971. The building was purchased by the U.S. Postal service in 1972. The building was designed by Vlasimil Koubek, a local architect. The building is part of the larger L'Enfant Plaza complex, which includes the Astral Building (North Building) (1968), Comsat Building (South Building) (1965), Loew's L'Enfant Plaza Hotel (East Building) (1971 to 1973), and the plaza.<sup>16</sup> L'Enfant Plaza was a major feature of the urban renewal of the southwest quadrant of Washington, DC, that took place during the mid-20<sup>th</sup>

**Figure 3-45** | USPS Building (Google Maps)



<sup>15</sup> Francesca Russello Ammon, *Historic American Buildings Survey, Southwest Washington Urban Renewal Area, HABS DC-856* (2004), 92.

<sup>16</sup> Francesca Russello Ammon, *Historic American Buildings Survey, Southwest Washington Urban Renewal Area, HABS DC-856* (2004), 92.

century, and is an interesting example of the Brutalist style in Washington, DC. The building appears to maintain sufficient integrity of location, design, setting, materials, workmanship, feeling and association to convey its significance. As such, the property is potentially eligible for listing in the NRHP.

#### **6. 398 Long Bridge Drive**

*Location:* 398 Long Bridge Drive, Arlington, VA

*Date of Construction:* 1957

A brick-clad commercial building. The building is composed of a two-story entrance block, and large, one-story warehouse space. The building's façade appears to have undergone several alterations, including changes to the fenestration, window replacement, main entrance alteration, and the addition of first floor awnings. The building appears to lack historic significance and integrity and is likely not eligible for listing in the NRHP.

**Figure 3-46** | 398 Long Bridge Drive  
(Google Maps)

